



30 June 2026

The Chief Executive Officer
Brisbane City Council
GPO Box 1434
Brisbane QLD 4000

Attention: Bijal Shah, Senior Urban Planner

Dear Bijal,

RESPONSE TO BRISBANE CITY COUNCIL FURTHER ADVICE LETTER

DEVELOPMENT PERMIT FOR MATERIAL CHANGE OF USE FOR MULTIPLE DWELLING OVER LAND AT 63 – 69 MELTON ROAD, NUNDAH (LOT 2 ON RP121656 & LOT 19 – 20 ON RP33945).

COUNCIL REFERENCE: A006988485

Mewing Planning Consultants act on behalf of Botega Property Commercial Pty Ltd and Girlshold Pty Ltd (**the Applicant**) in relation to the site at 63 – 69 Melton Road, Nundah (**the site**).

We refer to the correspondence from Brisbane City Council dated 16 June 2026 constituting a Further Advice Letter.

Please accept this correspondence, on behalf of the Applicant, as a response to the Further Advice Letter, providing additional information in respect of the development application. We advise that Brisbane City Council should proceed with the assessment of the development application on the basis of the new information present in this response.

Changes made to the development, as part of this correspondence, have been made in direct Response to Council's Further Advice letter.

The following attached response extracts each part of Council's Further Advice Letter and provides a corresponding response. The response includes the following documentation:

- **Attachment A** – Revised Architectural Plans, prepared by DAH Architecture; and
- **Attachment B** – Revised Traffic Assessment, prepared by ITE Consulting.

We would welcome the opportunity to discuss any aspect of this Further Advice Response. Should you wish to discuss, please contact Sam Ireland on 0468 933 603 or at sam.ireland@mewing.com.au.

Yours sincerely,

Sam Ireland
Graduate Town Planner
Mewing Planning Consultants

Introduction

The following correspondence provides a response to Brisbane City Council's (**Council**) Further Advice Letter dated 16 June 2026. The correspondence has extracted each part of Council's Further Advice Letter in italicised text and provides a corresponding response below.

At the outset, we note that the Applicant has made various amendments in response to Council's comments, including through engagement with Council officers. The following provides context to the proposed changes (with further detail in the specific item responses provided below).

Refuse Storage Area, Driveway Interface and Levels

Item 1

The submitted plans (including Drawing SD109 and associated sections) have been assessed, and it is identified that annotated level discrepancies between these elements, resulting in:

- a. *A vertical step (up to approximately 300mm) between the shared access area and driveway;*
- b. *Creation of a hazard for pedestrian movement and bin handling;*
- c. *Inability to achieve the demonstrated swept path performance for refuse collection vehicles (RCVs); and*
- d. *Reduction in effective trafficable driveway width.*

Provide amended plans (including sections and AHD spot levels) demonstrating that the refuse storage area, adjacent access pathway, and internal driveway are designed at consistent and compatible grades to ensure safe and functional operation for both pedestrian and refuse bin movements.

Amended plans must demonstrate a coordinated and functional design outcome that does not compromise either refuse servicing or driveway operation, including consistency across all submitted drawings in accordance with PO19 of the Transport, Access, Parking and Servicing code, PO32 of the Multiple dwelling code, and PO8 of the Infrastructure design code.

Item 1 Response

In response to **Item 1** of Council's Further Advice, the Applicant confirms that the shared pedestrian pathway provides a gradient consistent and compatible with the driveway. Refer to **Figure 1** below.

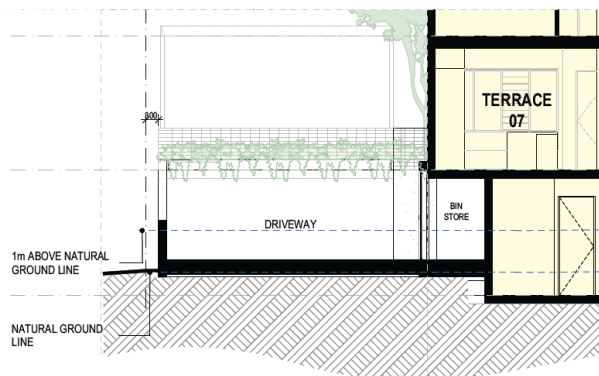


Figure 1: Section – gradient of shared pathway and driveway.

Source: DAH Architecture, 2026

Refer to the Revised Architectural Plans prepared by DAH Architecture and included in **Attachment A** for further detail.

Refuse Storage Area, Driveway Interface and Levels

Item 2

The current documentation demonstrates that proposed landscaping encroaches into the required swept path for RCV manoeuvring.

To ensure compliance, either:

- Remove the landscaping within the swept path envelope; or
- Limit landscaping to low ground cover that does not exceed 150mm in height and does not obstruct vehicle movement.

Provide revised swept path analysis and landscape plans that resolve the identified conflict along the northern edge of the internal driveway.

All plans must be consistent and clearly demonstrate unobstructed vehicle manoeuvrability in accordance with PO19 of the Transport, Access, Parking and Servicing code and PO32 of the Multiple dwelling code.

Item 2 Response

In response to **Item 2** of Council's Further Advice, the landscape strip adjacent to the driveway has been removed. As outlined in **Figure 2** below, the removal of the landscape strip ensures that no conflicts with the RCV manoeuvring occurs.

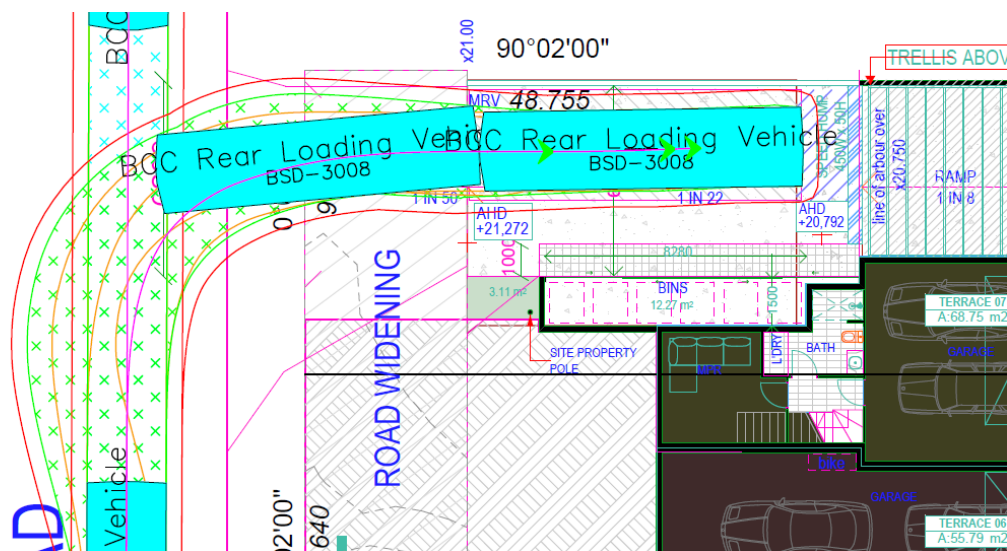


Figure 2: Revised swept path for RCV manoeuvring.

Source: ITE Consulting, 2026

Refer to the Traffic Impact Assessment prepared by ITE Consulting and included in **Attachment B** for further detail.

Refuse Storage Area, Driveway Interface and Levels

Item 3

The location of the proposed speed control device (speed bump) is inconsistent across submitted plans and does not achieve its intended function of preventing bins from rolling down the adjoining steep driveway (up to 1:8 grade).

For a detailed response to **Item 4** of Council's Further Advice, please refer to the Revised Traffic Impact Assessment prepared by ITE Consulting and included in **Attachment B**.

Refuse Storage Area, Driveway Interface and Levels

Item 5

The revised civil drawings still show a retaining wall in cut against the northern property boundary without an appropriate offset to allow for construction and the installation of subsoil drainage within the property.

- Provide detailed design documentation and supporting commentary demonstrating how the proposed retaining wall located in cut along the adjoining property boundary will be constructed without encroachment onto, or impact to, the neighbouring property.
- Alternatively, revise the design to setback the retaining wall a minimum of 300mm from the boundary to allow for construction access and subsoil drainage.

Details must include construction methodology, maintenance considerations, and drainage provisions in accordance with PO2 of the Filling and excavation code.

Item 5 Response

In response to **Item 5** of Council's Further Advice, the retaining wall along the northern boundary has been setback 300mm off the northern side boundary. Refer to **Figure 4** below.

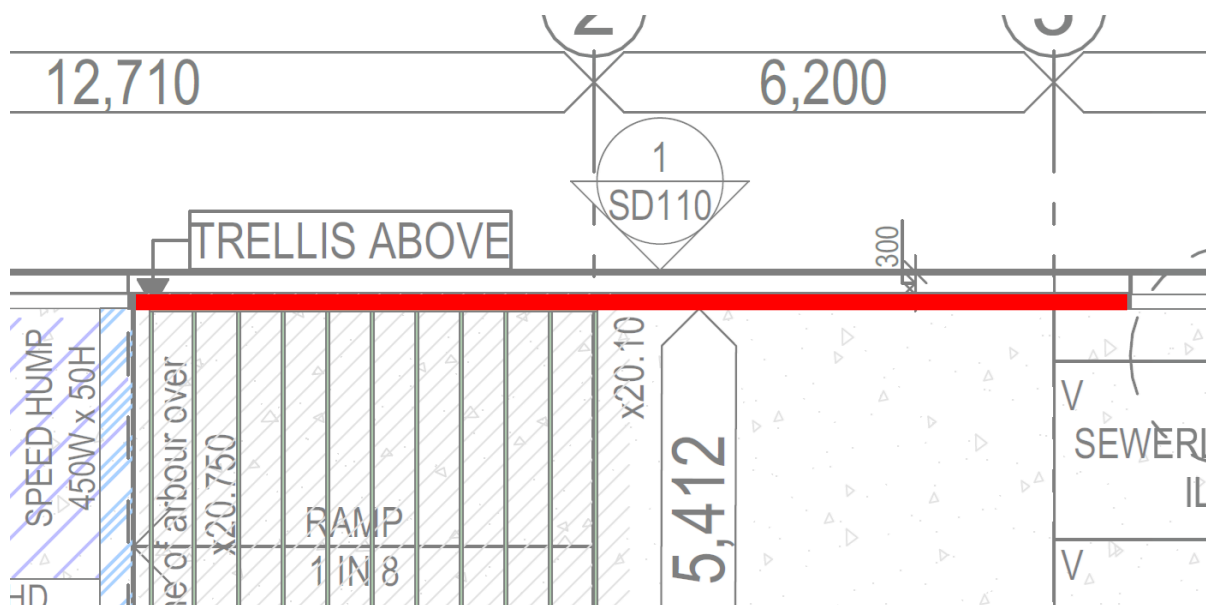


Figure 4: Revised Retaining Wall Outcome (retaining wall outlined in red)

Source: DAH Architecture, 2026

Refer to the Revised Architectural Plans prepared by DAH Architecture and included in **Attachment B** for further detail.