

# DEVELOPMENT APPLICATION CIVIL ENGINEERING

## PROPOSED DEVELOPMENT

50 KATES STREET, MORNINGSID

LOCAL AUTHORITY: BRISBANE CITY COUNCIL

DATE PREPARED: 29<sup>TH</sup> JANUARY, 2026

REVISION NO: 0

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# INFRASTRUCTURE DESIGN CODE & STORMWATER MANAGEMENT CODE

CODE COMPLIANCE

**Brisbane City Plan 2014**

*Performance Criteria and Acceptable Solutions*

**Solutions:** A- Acceptable Solution, OS – Other/Alternative Solution, NA – Not Applicable

PERFORMANCE CRITERIA	ACCEPTABLE SOLUTIONS	SOLUTIONS	COMPLIANCE	COUNCIL USE
<b>9.4.4 Infrastructure Design Code</b>				
<p><b>PO1</b>  <i>Development provides roads, pavement, edging and landscaping which:</i>  <i>(a) are designed and constructed in accordance with the road hierarchy;</i>  <i>(b) provide for safe travel for pedestrians, cyclists and vehicles;</i>  <i>(c) provide access to properties for all modes;</i>  <i>(d) provide utilities;</i>  <i>(e) provide high levels of aesthetics and amenity, improved livability and future growth;</i>  <i>(f) provide for the amelioration of noise and other pollution;</i>  <i>(g) provide a high-quality streetscape;</i>  <i>(h) provide a low-maintenance asset with a minimal whole-of-life cost.</i>  <i>Note—This can be demonstrated in an engineering report prepared and certified by a <a href="#">Registered Professional Engineer Queensland</a> in accordance with the <a href="#">Infrastructure design planning scheme policy</a>.</i></p>	<p><b>AO1</b>  <i>Development provides roads and associated pavement, edging and landscaping which are designed and constructed in compliance with the road corridor design standards in the <a href="#">Infrastructure design planning scheme policy</a>.</i></p>	<p><b>A</b></p>	<p><i>Any proposed associated pavement, edging and landscaping if required will designed in accordance with the Infrastructure Design Planning Scheme Policy.</i></p> <p><i>All existing pavement, edging and landscaping is satisfactory and acceptable.</i></p>	

<p><b>PO2</b> Development provides road pavement surfaces which: (a) are well designed and constructed; (b) durable enough to carry the wheel loads of the intended types and numbers of travelling and parked vehicles; (c) ensures the safe passage of vehicles, pedestrians and cyclists, the discharge of stormwater run-off and the preservation of all-weather access; (d) allows for reasonable travel comfort.</p>	<p><b>A02</b> Development provides road pavement surfaces which are designed and constructed in compliance with the road corridor design standards in the <a href="#">Infrastructure design planning scheme policy</a>.</p>	<p><b>A</b></p>	<p>If any proposed road pavement surfaces are required, then they will be designed and constructed in accordance with the Infrastructure Design Planning Scheme Policy. All existing road pavement surfaces are satisfactory and acceptable</p>	
<p><b>PO3</b> Development provides a pavement edge which is designed and constructed to: (a) control vehicle movements by delineating the carriageway for all users; (b) provide for people with disabilities by allowing safe passage of wheelchairs and other mobility aids.</p>	<p><b>A03</b> Development provides pavement edges which are designed and constructed in compliance with the road corridor design standards in the <a href="#">Infrastructure design planning scheme policy</a>.</p>	<p><b>A</b></p>	<p>If any proposed pavement edges are required, then they will be designed and constructed in accordance with the Infrastructure Design Planning Scheme Policy. All existing pavement edges are satisfactory and acceptable.</p>	
<p><b>PO4</b> Development provides verges which are designed and constructed to: (a) provide safe access for pedestrians clear of obstructions and access areas for vehicles onto properties; (b) provide a sufficient area for public utility services; (c) be maintainable by the Council.</p>	<p><b>A04</b> Development provides verges which are designed and constructed in compliance with the road corridor design and streetscape locality advice standards in the <a href="#">Infrastructure design planning scheme policy</a>.</p>	<p><b>A</b></p>	<p>If any proposed verges are required, then they will be designed and constructed in accordance with the Infrastructure Design Planning Scheme Policy.  All existing verges are satisfactory and acceptable</p>	

<p><b>PO5</b>  <i>Development provides a lane or laneway identified in a neighbourhood plan which:</i>  <i>(a) allows equitable access for all modes;</i>  <i>(b) is safe and secure;</i>  <i>(c) has 24-hour access;</i>  <i>(d) is a low-speed shared zone environment;</i>  <i>(e) has a high-quality streetscape.</i></p>	<p><b>A05</b>  <i>Development provides a lane or laneway identified in a neighbourhood plan which is embellished in compliance with the streetscape locality advice standards in the <a href="#">Infrastructure design planning scheme policy</a>.</i></p>	<p>N/A</p>		
<p><b>PO6</b>  <i>Development of an existing premises provides at the frontage to the site, if not already provided, the following infrastructure to an appropriate urban standard:</i>  <i>(a) an effective, high-quality paved roadway;</i>  <i>(b) an effective, high-quality roadway kerb and channel;</i>  <i>(c) safe, high-quality vehicle crossings over channels and verges;</i>  <i>(d) safe, accessible, high-quality verges compatible and integrated with the surrounding environment;</i>  <i>(e) safe vehicle access to the site that enables ingress and egress in a forward gear;</i>  <i>(f) provision of and required alterations to public utilities;</i>  <i>(g) effective drainage;</i>  <i>(h) appropriate conduits to facilitate the provision of required street-lighting systems and traffic signals.</i></p>	<p><b>A06</b>  <i>Development of an existing premises provides at the frontage of the site, if not already existing, the following infrastructure to the standard that would have applied if the development involved new premises as stated in the road corridor design standards in the <a href="#">Infrastructure design planning scheme policy</a>:</i>  <i>(a) concrete kerb and channel;</i>  <i>(b) forming and grading to verges;</i>  <i>(c) crossings over channels and verges;</i>  <i>(d) a constructed bikeway;</i>  <i>(e) a constructed verge or reconstruction of any damaged verge;</i>  <i>(f) construction of the carriageway;</i>  <i>(g) payment of costs for required alterations to public utility mains, services or installations;</i>  <i>(h) construction of and required alterations to public utility mains, services or installations;</i>  <i>(i) drainage works;</i>  <i>(j) installation of electrical conduits.</i></p>	<p>A</p>	<p><i>An acceptable solution can be achieved as the frontage works have previously been constructed and are generally in good condition.</i></p> <p><i>All new driveway crossings will be constructed in accordance with the Infrastructure Design Planning Scheme Policy. All new concrete footpaths will be constructed in accordance with the Infrastructure Design Planning Scheme Policy.</i></p> <p><i>Redundant vehicular crossings and kerb outlets will be removed and the footpath and kerb will be re-instated in accordance with the Infrastructure Design Planning Scheme Policy.</i></p> <p><i>Service Conduit and mains will be installed as required in accordance with the Infrastructure Design Planning Scheme Policy.</i></p>	

<p><b>PO7</b> Development provides both cycle and walking routes which: (a) are located, designed and constructed to their network classification (where applicable); (b) provide safe and attractive travel routes for pedestrians and cyclists for commuter and recreational purposes; (c) provide safe and comfortable access to properties for pedestrians and cyclists; (d) incorporate water sensitive urban design into stormwater drainage; (e) provide for utilities; (f) provide for a high level of aesthetics and amenity, improved livability and future growth; (g) are a low-maintenance asset with a minimal whole-of-life cost; (h) minimise the clearing of significant native vegetation. Note—This can be demonstrated in an engineering report prepared and certified by a <a href="#">Registered Professional Engineer Queensland</a> in accordance with the <a href="#">Infrastructure design planning scheme policy</a>.</p>	<p><b>A07</b> Development provides cycle and walking routes which are located, designed and constructed in compliance with the road corridor design and off-road pathway design standards in the <a href="#">Infrastructure design planning scheme policy</a>.</p>	<p>N/A</p>		
<p><b>PO8</b> Development provides refuse and recycling collection, separation and storage facilities that are located and managed so that adverse impacts on building occupants, neighbouring properties and the public realm are minimised.</p>	<p><b>A08.1</b> Development provides refuse and recycling collection and storage facilities in accordance with the <a href="#">Refuse planning scheme policy</a>.</p> <p><b>A08.2</b> Development ensures that refuse and recycling collection and storage location and design do not have any adverse impact</p>	<p>A</p> <p>A</p>	<p>Any proposed refuse and recycling collection and storage facilities (if required) will be detailed by the relevant approved professional</p> <p>Any proposed refuse and recycling collection and storage facilities locations (if required) will be detailed by the relevant approved professional</p>	



<p><b>PO10</b> Development provides public utilities and street lighting which are the best current or alternative technology and facilitate accessibility, easy maintenance, minimal whole-of-life costs, and minimal adverse environmental impacts.</p>	<p><b>AO10.1</b> Development provides public utilities and street lighting which are located and aligned to: (a) avoid significant native vegetation and areas identified within the <a href="#">Biodiversity areas overlay map</a>; (b) minimise earthworks; (c) avoid crossing waterways, waterway corridors and wetlands or if a crossing is unavoidable, tunnel-boring techniques are used to minimise disturbance, and a disturbed area is reinstated and restored on completion of the work. Note—Guidance on the restoration of habitat is included in the <a href="#">Biodiversity areas planning scheme policy</a>.</p> <p><b>AO10.2</b> Development provides compatible public utility services and street-lighting services which are co-located in common trenching for underground services.</p> <p><b>AO10.3</b> Development provides public utilities and street lighting which are designed and constructed in compliance with the public utilities standards in the <a href="#">Infrastructure design planning scheme policy</a>.</p>	<p><b>A</b></p> <p><b>A</b></p> <p><b>A</b></p> <p><b>A</b></p>	<p><b>Electricity services</b> Electricity services are readily accessible by way of existing overhead reticulation adjacent to the site. If required a design will be provided by the relevant utilities supplier or the appropriate services sub-contractor for the required Electrical, Telecommunications and Gas Supply</p> <p><b>Telephone services</b> Telephone services are readily accessible by way of existing underground reticulation adjacent to the site. If required a design will be provided by the relevant utilities supplier or the appropriate services sub-contractor for the required Electrical, Telecommunications and Gas Supply</p> <p>Service Conduits and mains will be installed if required in accordance with the Infrastructure Design Planning Scheme Policy.</p> <p>All proposed public utilities and street lighting will be designed and constructed in accordance with the Infrastructure Design Planning Scheme Policy.</p>	
<p><b>PO11</b> Development ensures that land used for urban purposes is serviced adequately with telecommunications and energy supply.</p>	<p><b>AO11</b> Development provides land with the following services to the standards of the approved supplier: (a) electricity; (b) telecommunications services; (c) gas service where practicable.</p>	<p><b>A</b></p>	<p>A Certificate of Supply will be provided by the relevant utilities supplier or the appropriate services sub-contractor for the required Electrical, Telecommunications and Gas Supply.</p>	

<p><b>PO12</b> Development ensures that major public projects promote the provision of affordable, high-bandwidth telecommunications services throughout the city.</p>	<p><b>AO12</b> Development provides conduits which are provided in all major Council and government works projects to enable the future provision of fibre optic cabling, if: (a) the additional expense is unlikely to be prohibitive; or (b) further major work is unlikely or disruption would be a major concern, such as where there is a limited capacity road; or (c) there is a clear gap in the telecommunications network; or (d) there is a clear gap in the bandwidth available to the area. Editor's note—An accurate, digital 'as built' three-dimensional location plan is to be supplied for all infrastructure provided in a road</p>	<p><b>A</b></p>	<p>If required the proposed development, will provide conduits for future fibre optic cabling.</p>	
<p><b>PO13</b> Development provides public art identified in a neighbourhood plan or park concept plan which: (a) is provided commensurate with the status and scale of the proposed development; (b) is sited and designed: (i) as an integrated part of the project design; (ii) as conceptually relevant to the context of the location; (iii) to reflect and respond to the cultural values of the community; (iv) to promote local character in a planned and informed manner.</p>	<p><b>AO13</b> Development provides public art identified in a neighbourhood plan or <a href="#">park concept plan</a> which is sited and designed in compliance with the public art standards in the <a href="#">Infrastructure design planning scheme policy</a>.</p>	<p><b>NA</b></p>		

<p><b>PO14</b> Development provides signage of buildings and spaces which promote legibility to help users find their way.</p>	<p><b>AO14</b> Development provides public signage: (a) at public transport interchanges and stops, key destinations, public spaces, pedestrian linkages and at entries to centre developments; (b) which details the location of the key destinations, public spaces and pedestrian linkages in the vicinity, the services available within the development and where they are located. Editor's note—Signage is to be in accordance with <a href="#">Local Law Number 1 (Control of Advertisements Local Law)</a>.</p>	NA		
<p><b>PO15</b> Development that provides community facilities which form part of the development is functional, safe, low maintenance, and fit for purpose.</p>	<p><b>AO15</b> Development that provides community facilities which form part of the development is designed in compliance with the community facilities standards in the <a href="#">Infrastructure design planning scheme policy</a>.</p>	NA		
<p><b>PO16</b> Development provides public toilets which: (a) are required as part of a community facility or park; (b) are located, designed and constructed to be: (i) safe; (ii) durable; (iii) resistant to vandalism; (iv) able to service expected demand; (v) fit for purpose.</p>	<p><b>AO16</b> Development that provides public toilets is designed and constructed in compliance with the public toilets standards in the <a href="#">Infrastructure design planning scheme policy</a>.</p>	NA		

<p><b>PO17</b>  <i>Development provides bridges, tunnels, elevated structures and water access structures that are designed and constructed using proven methods, materials and technology to provide for:</i>  <i>(a) safe movement of intended users;</i>  <i>(b) an attractive appearance appropriate to the general surroundings and any adjacent structures;</i>  <i>(c) functionality and easy maintenance;</i>  <i>(d) minimal whole-of-life cost;</i>  <i>(e) longevity;</i>  <i>(f) current and future services.</i>  <i>Note—All bridges and elevated and associated elements must be designed and certified by a <a href="#">Registered Professional Engineer Queensland</a> in accordance with the <a href="#">Infrastructure design planning scheme policy</a>.</i></p>	<p><b>AO17</b>  <i>Development that provides bridges, tunnels, elevated structures and water access structures is designed and constructed in compliance with the standards in the <a href="#">Infrastructure design planning scheme policy</a>.</i></p>	<p><b>NA</b></p>		
<p><b>PO18</b>  <i>Development provides culverts which are designed and constructed using proven methods, materials and technology to provide for:</i>  <i>(a) safety;</i>  <i>(b) an attractive appearance appropriate to the general surroundings;</i>  <i>(c) functionality and easy maintenance;</i>  <i>(d) minimal whole-of-life cost;</i>  <i>(e) longevity;</i>  <i>(f) future widening;</i>  <i>(g) current and future services;</i>  <i>(h) minimal adverse impacts, such as increase in water levels or flow velocities, and significant change of flood patterns.</i>  <i>Note—All culverts and associated</i></p>	<p><b>AO18</b>  <i>Development that provides culverts is designed and constructed in compliance with the structures standards in the <a href="#">Infrastructure design planning scheme policy</a>.</i></p>	<p><b>N/A</b></p>		

<p>elements are to be designed and certified by a <a href="#">Registered Professional Engineer Queensland</a> in accordance with the applicable design standards.</p>				
<p><b>PO19</b> Development provides batters, retaining walls, and seawalls and river walls which are designed and constructed using proven methods, materials and technology to provide for: (a) safety; (b) an attractive appearance appropriate to the surrounding area; (c) easy maintenance; (d) minimal whole-of-life cost; (e) longevity; (f) minimal water seepage. Note—All retaining walls and associated elements are to be designed and certified by a <a href="#">Registered Professional Engineer Queensland</a> in accordance with the applicable design standards.</p>	<p><b>AO19</b> Development that provides batters, retaining walls, seawalls and river walls is designed and constructed in compliance with the structures standards in the <a href="#">Infrastructure design planning scheme policy</a>.</p>	<p><b>A</b></p>	<p>Any proposed batters, retaining walls, seawalls and river walls will designed and constructed in compliance with the structures standards in the <a href="#">Infrastructure design planning scheme policy</a>.</p>	
<p><b>PO20</b> Development ensures that construction is managed so that use of public spaces and movement on pedestrian, cyclist and other traffic routes is not unreasonably disrupted and existing landscaping is adequately protected from short- and long-term impacts. Note—The preparation of a construction management plan can assist in demonstrating achievement of this performance outcome. Note—The <a href="#">Transport, access, parking and servicing planning scheme policy</a> provides advice on the management of vehicle parking and deliveries during construction</p>	<p><b>AO20</b> Development ensures that during construction: (a) the ongoing use of adjoining and surrounding parks and public spaces, such as malls and outdoor dining, is not compromised; (b) adjoining and surrounding landscaping is protected from damage; (c) safe, legible, efficient and sufficient pedestrian, cyclist and vehicular accessibility and connectivity to the wider network are maintained.</p>	<p><b>A</b></p>	<p>During any proposed construction works: The ongoing use of adjoining and surrounding parks and public spaces, such as malls and outdoor dining, will not be compromised; Adjoining and surrounding landscaping will be protected from damage; and Safe, legible, efficient and sufficient pedestrian, cyclist and vehicular accessibility and connectivity will be provided to the wider network.</p>	



## Brisbane City Plan 2014

## Performance Criteria and Acceptable Solutions

Solutions: A- Acceptable Solution, OS – Other/Alternative Solution, NA – Not Applicable

PERFORMANCE CRITERIA	ACCEPTABLE SOLUTIONS	SOLUTIONS	COMPLIANCE	COUNCIL USE
<b>9.4.9 Stormwater Code</b>				
<p><b>PO1</b> Development provides a stormwater management system which achieves the integrated management of stormwater to:</p> <ul style="list-style-type: none"> <li>(a) minimise flooding;</li> <li>(b) protect environmental values of receiving waters;</li> <li>(c) maximise the use of water sensitive urban design;</li> <li>(d) minimise safety risk to all persons;</li> <li>(e) maximise the use of natural waterway corridors and natural channel design principles.</li> </ul> <p>Editor's note—The stormwater management system to be developed to address PO1 is not intended to require management of stormwater quality.</p>	<p><b>AO1</b> <i>Development provides a stormwater management system designed in compliance with the <a href="#">Infrastructure design planning scheme policy</a>.</i></p>	<p><b>A</b></p>	<p><i>A Site Based Stormwater Management Plan has been prepared for the site and incorporates drainage infrastructure required to satisfy the intent of the Stormwater Management Code.</i></p>	
<p><b>PO2</b> Development ensures that the stormwater management system and site work does not adversely impact flooding or drainage characteristics of premises which are up slope, down slope or adjacent to the site.</p>	<p><b>AO2.1</b> <i>Development does not result in an increase in flood level or flood hazard on up slope, down slope or adjacent premises.</i></p> <p><b>AO2.2</b> <i>Development provides a stormwater management system which is designed in compliance with the standards in the <a href="#">Infrastructure design planning scheme policy</a>.</i></p>	<p><b>A</b></p> <p><b>A</b></p>	<p><i>There is no increase in flood level on upslope, downslope or adjacent premises. The site is also not flood affected.</i></p> <p><i>Proposed stormwater drainage infrastructure will be designed and constructed to satisfy the intent of the Stormwater Management Code.</i></p> <p><i>Runoff from the developed areas of this property be discharged in a conventional manner as detailed in this Report and in accordance with the Infrastructure Design Planning Scheme Policy.</i></p>	

<p><b>PO3</b> Development ensures that the stormwater management system does not direct stormwater run-off through existing or proposed lots and property where it is likely to adversely affect the safety of, or cause nuisance to properties.</p>	<p><b>A03.1</b> <i>Development ensures that the location of the stormwater drainage system is contained within a road reserve, drainage reserve, public pathway, park or waterway corridor.</i></p> <p><b>A03.2</b> <i>Development provides a stormwater management system which is designed in compliance with the standards in the <a href="#">Infrastructure design planning scheme policy</a>.</i></p> <p><b>A03.3</b> <i>Development obtains a lawful point of discharge in compliance with the standards in the <a href="#">Infrastructure design planning scheme policy</a>.</i></p> <p><b>A03.4</b> <i>Where on private land, all underground stormwater infrastructure is secured by a drainage easement.</i></p>	<p><b>A</b></p> <p><b>A</b></p> <p><b>A</b></p> <p><b>A</b></p>	<p><i>Runoff from developed areas can be captured and discharged to the existing downstream infrastructure located at the rear of the site in accordance with the Infrastructure Design Planning Scheme Policy.</i></p> <p><i>Runoff from developed areas can be captured and discharged to the existing downstream infrastructure located at the rear of the site in accordance with the Infrastructure Design Planning Scheme Policy.</i></p> <p><i>Runoff from developed areas can be captured and discharged to the existing downstream infrastructure located at the rear of the site in accordance with the Infrastructure Design Planning Scheme Policy.</i></p> <p><i>A drainage easement will be provided over any proposed Stormwater Pipe over 150mm in diameter and if the rear neighbour is in agreement.</i></p>	
<p><b>PO4</b> Development provides a stormwater management system which has sufficient capacity to safely convey run-off taking into account increased run-off from impervious surfaces and flooding in local catchments.</p>	<p><b>A04.1</b> <i>Development provides a stormwater conveyance system which is designed to safely convey flows in compliance with the standards in the <a href="#">Infrastructure design planning scheme policy</a>.</i></p> <p><b>A04.2</b> <i>Development provides sufficient area to convey run-off which will comply with the standards in the <a href="#">Infrastructure design planning scheme policy</a>.</i></p>	<p><b>A</b></p> <p><b>A</b></p>	<p><i>Runoff from developed areas can be captured and discharged to the existing downstream infrastructure located at the rear of the site in accordance with the Infrastructure Design Planning Scheme Policy.</i></p> <p><i>All run-off will comply with the standards in the <a href="#">Infrastructure design planning scheme policy</a>.</i></p>	

<p><b>PO5</b> Development designs stormwater channels, creek modification works, bridges, culverts and major drains to protect and enhance the value of the waterway corridor or drainage path for fauna movement.</p>	<p><b>A05</b> <i>Development ensures the design of stormwater channels, creek modifications or other infrastructure, permits terrestrial and aquatic fauna movement.</i></p>	<p><b>NA</b></p>		
<p><b>PO6</b> Development ensures that location and design of stormwater detention and water quality treatment: (a) minimises risk to people and property; (b) provides for safe access and maintenance; (c) minimises ecological impacts to creeks and waterways.</p>	<p><b>A06.1</b> <i>Development locates stormwater detention and water quality treatment:</i> <i>(a) outside of a waterway corridor;</i> <i>(b) offline to any catchment not contained within the development.</i></p> <p><b>A06.2</b> <i>Development providing for stormwater detention and water quality treatment devices are designed in compliance with the standards in the <a href="#">Infrastructure design planning scheme policy</a>.</i></p>	<p><b>NA</b></p> <p><b>NA</b></p>		
<p><b>PO7</b> Development is designed, including any car parking areas and channel works to: (a) reduce property damage; (b) provide safe access to the site during the <a href="#">defined flood event</a>.</p>	<p><b>A07.1</b> <i>Development (including any ancillary structures and car parking areas) is located above minimum flood immunity levels in <a href="#">Table 9.4.9.3.B</a>, <a href="#">Table 9.4.9.3.C</a>, <a href="#">Table 9.4.9.3.D</a>, <a href="#">Table 9.4.9.3.E</a> and <a href="#">Table 9.4.9.3.F</a>. Note—Compliance with this acceptable outcome can be demonstrated by the submission of a hydraulic and hydrology report identifying flood levels and development design levels (as part of a site-based stormwater management plan).</i></p> <p><b>A07.2</b> <i>Development including the road network provides a stormwater management system that provides safe pedestrian and vehicle access in accordance with the standards in</i></p>	<p><b>NA</b></p> <p><b>NA</b></p>	<p><i>The site is not flood affected.</i></p>	

	the <a href="#">Infrastructure design planning scheme policy</a> .			
<p><b>PO8</b> Development designs stormwater channels, creek modification works and the drainage network to protect and enhance the environmental values of the waterway corridor or drainage path.</p>	<p><b>A08.1</b> <i>Development ensures natural waterway corridors and drainage paths are retained.</i></p> <p><b>A08.2</b> <i>Development provides the required hydraulic conveyance of the drainage channel and floodway, while maximising its potential to maximise environmental benefits and minimise scour.</i> <i>Editor's note—Guidance on natural channel design principles can be found in the Council's publication <a href="#">Natural channel design guidelines</a>.</i></p> <p><b>A08.3</b> <i>Development provides stormwater outlets into waterways, creeks, wetlands and overland flow paths with energy dissipation to minimise scour in compliance with the standards in the <a href="#">Infrastructure design planning scheme policy</a>.</i></p>	<p><b>NA</b></p> <p><b>NA</b></p> <p><b>NA</b></p>		
<p><b>PO9</b> Development is designed to manage run-off and peak flows by minimising large areas of impervious material and maximising opportunities for capture and re-use.</p>	<p><b>A09</b> <i>No acceptable outcome is prescribed.</i></p>	<p><b>A</b></p>	<p><i>A Site Based Stormwater Management Plan has been prepared for the site and incorporates drainage infrastructure required to satisfy the intent of the Stormwater Management Code.</i></p>	
<p><b>PO10</b> Development ensures that there is sufficient site area to accommodate an effective stormwater management system. Note—Compliance with the</p>	<p><b>A010</b> <i>No acceptable outcome is prescribed.</i></p>	<p><b>A</b></p>	<p><i>A Site Based Stormwater Management Plan has been prepared for the site and incorporates drainage infrastructure required to satisfy the intent of the Stormwater Management Code.</i></p>	

<p>performance outcome should be demonstrated by the submission of a site-based stormwater management plan for high-risk development only</p>				
<p><b>PO11</b> Development provides for the orderly development of stormwater infrastructure within a catchment, having regard to the: (a) existing capacity of stormwater infrastructure within and external to the site, and any planned stormwater infrastructure upgrades; (b) safe management of stormwater discharge from existing and future up-slope development; (c) implication for adjacent and down-slope development.</p>	<p><b>AO11.1</b> <i>Development with up-slope external catchment areas provides a drainage connection sized for ultimate catchment conditions that is directed to a lawful point of discharge.</i></p> <p><b>AO11.2</b> <i>Development ensures that existing stormwater infrastructure that is undersized is upgraded in compliance with the <a href="#">Priority infrastructure plan</a> and the standards in the <a href="#">Infrastructure design planning scheme policy</a></i></p>	<p><b>A</b></p> <p><b>A</b></p>	<p><i>No upslope catchment applies to this development. No extra stormwater infrastructure will be required in accordance with Infrastructure Design planning scheme policy 7.6.5</i></p> <p><i>Any required upgrades will be done in accordance with the Infrastructure Design Planning Scheme Policy.</i></p>	
<p><b>PO12</b> Development provides stormwater infrastructure which: (a) remains fit for purpose for the life of the development and maintains full functionality in the design flood event; (b) can be safely accessed and maintained cost effectively; (c) ensures no structural damage to existing stormwater infrastructure</p>	<p><b>AO12.1</b> <i>The stormwater management system is designed in compliance with the <a href="#">Infrastructure design planning scheme policy</a>.</i></p> <p><b>AO12.2</b> <i>Development provides a clear area with a minimum of 2m radius from the centre of an existing manhole cover and with a minimum height clearance of 2.5m.</i></p>	<p><b>A</b></p> <p><b>A</b></p>	<p><i>The stormwater management system will be designed in accordance with the Infrastructure Design Planning Scheme Policy.</i></p> <p><i>A clear area with a radius of 2m will be provided for any existing or proposed manhole covers. A vertical clearance of 2.5 will be provided if required.</i></p>	
<p><b>PO13</b> <i>Development ensures that all reasonable and practicable measures are taken to manage the impacts of erosion, turbidity and sedimentation, both within and external to the development site from construction activities, including vegetation clearing, earthworks, civil construction, installation of services,</i></p>	<p><b>AO13</b> <i>No acceptable outcome is prescribed.</i></p>	<p><b>A</b></p>	<p><i>An acceptable solution will be achieved through the implementation of a Sediment &amp; Erosion Control program that requires the establishment and maintenance of controls in accordance with the Infrastructure Design Planning Scheme Policy.</i></p>	

<p>rehabilitation, revegetation and landscaping to protect:</p> <p>(a) the environmental values and water quality objectives of waters;</p> <p>(b) waterway hydrology;</p> <p>(c) the maintenance and serviceability of stormwater infrastructure.</p> <p>Note—The <a href="#">Infrastructure design planning scheme policy</a> outlines the appropriate measures to be taken into account to achieve the performance outcome.</p>				
<p><b>PO14</b></p> <p>Development ensures that:</p> <p>(a) unnecessary disturbance to soil, waterways or drainage channels is avoided;</p> <p>(b) all soil surfaces remain effectively stabilised against erosion in the short and long term.</p>	<p><b>AO14</b></p> <p>No acceptable outcome is prescribed.</p>	<p><b>A</b></p>	<p>An acceptable solution will be achieved through the implementation of a Sediment &amp; Erosion Control program that requires the establishment and maintenance of controls in accordance with the Infrastructure Design Planning Scheme Policy.</p>	
<p><b>PO15</b></p> <p>Development does not increase:</p> <p>(a) the concentration of total suspended solids or other contaminants in stormwater flows during site construction;</p> <p>(b) run-off which causes erosion either on site or off site.</p>	<p><b>AO15</b></p> <p>No acceptable outcome is prescribed</p>	<p><b>A</b></p>	<p>The development will not increase the total amount of contaminants in the Stormwater Flows during construction. To avoid this a Sediment &amp; Erosion Control program that requires the establishment and maintenance of controls in accordance with the Infrastructure Design Planning Scheme Policy will be put in place.</p>	
<p><b>PO16</b></p> <p>Development ensures that the entry and transport of contaminants into stormwater is avoided or minimised to protect receiving water environmental values.</p> <p>Note—Prescribed water contaminants are defined in the <a href="#">Environmental Protection Act 1994</a>.</p> <p>Note—Compliance with the</p>	<p><b>AO16</b></p> <p>Development provides a stormwater management system which is designed in compliance with the standards in the <a href="#">Infrastructure design planning scheme policy</a>.</p>	<p><b>A</b></p>	<p>A stormwater management system will be designed with Best Practice stormwater quality in accordance with the Infrastructure Design Planning Scheme Policy</p>	

<p>performance outcome should be demonstrated by the submission of a site-based stormwater management plan for high-risk development only.</p>				
<p><b>PO17</b>          Development ensures that:          (a) the discharge of wastewater to a waterway or external to the site is avoided; or          (b) if the discharge cannot practicably be avoided, the development minimises wastewater discharge through re-use, recycling, recovery and treatment.          Note—The preparation of a wastewater management plan can assist in demonstrating achievement of this performance outcome.          Editor’s note—This code does not deal with sewerage which is the subject of the <a href="#">Wastewater code</a>.</p>	<p><b>AO17</b>  <i>No acceptable outcome is prescribed.</i></p>	<p><b>A</b></p>	<p><i>No wastewater will be discharged to a waterway or external to the site.</i></p>	

# FILLING AND EXCAVATION

CODE COMPLIANCE

Brisbane City Plan  
Filling and Excavation Code  
Compliance Outcomes

Compliance Outcomes	Proposal	Complies
<b>Section A – Conditions of a Development Permit</b>		
<p><b>CO1</b> <a href="#">Filling or excavation</a> is undertaken in accordance with the standards and requirements in the conditions of a development permit for the development.</p>	<p>Any required filling will comply with the standards and requirements in the conditions of a development permit for the development.</p>	<p>Y</p>
<b>Section B – If lodging a Development Application for Operational Work whether or not in conjunction with a Development Application for a Material Change of Use or Reconfiguring a Lot</b>		
<p><b>CO2</b> The total height of any cut and fill, whether or not retained, does not exceed: (a) 2.5m in a zone in the Industry zones category; (b) 1m in all other zones or if adjoining a sensitive zone.</p>	<p>The proposed filling may exceed 1m. However, the appropriate retaining measures will be put in place.</p>	<p>Y</p>
<p><b>CO3</b> <a href="#">Filling or excavation</a> does not involve disturbing more than 2,500m<sup>2</sup> of land and sediment and erosion control plan is prepared and implemented.</p>	<p>The development does not involve disturbing more than 2,500m<sup>2</sup> of land and sediment and erosion control plan will be prepared and implemented.</p>	<p>Y</p>
<p><b>CO4</b> <a href="#">Filling or excavation</a> does not involve the construction of open drainage.</p>	<p>There will be no open drainage on site</p>	<p>Y</p>
<p><b>CO5</b> A retaining structure including footings, surface drainage and subsoil drainage, is wholly contained within the site. If the total height to be retained is greater than 1m: (a) any retaining wall at the property boundary is no greater than 1m above <a href="#">ground level</a>; (b) all further terracing from the 1m high boundary retaining wall is 1 vertical unit: 1 horizontal unit; (c) the distance between each successive retaining wall (back of lower wall to face of higher wall) is no less than 1m horizontally and incorporates planting areas.</p>	<p>Any proposed retaining walls will be contained within the site and designed in accordance with the standards and requirements in the conditions of a development permit for the development.</p>	<p>Y</p>
<p><b>CO6</b> A retaining wall over 1m in height protects root growth of trees on the site and on adjoining land by avoiding cutting of roots.</p>	<p>Any trees remaining on site or in neighbouring lots will have roots protected for any future growth.</p>	<p>Y</p>

<p><b>CO7</b> A retaining wall finish that presents to adjoining land is maintenance free if the setback is less than 750mm from the boundary.</p>	<p>Any retaining walls will be set back the required distance and will be maintenance free for neighbouring properties.</p>	<p>Y</p>
<p><b>CO8</b> A retaining wall over 1m in height is designed and constructed in compliance with the structures section of the <a href="#">Infrastructure design planning scheme policy</a> and certified by a <a href="#">Registered Professional Engineer Queensland</a>.</p>	<p>Any retaining walls will designed and constructed in compliance with the structures section of the Infrastructure design planning scheme policy and certified by a Registered Professional Engineer Queensland.</p>	<p>Y</p>
<p><b>CO9</b> Filling only involves clean fill that does not include any construction rubble, debris, weed seed or viable parts of plant species listed as an undesirable plant species in the <a href="#">Planting species planning scheme policy</a>.</p>	<p>All fill transported to site will be free of construction rubble, debris, weed seed or viable parts of plant species listed as an undesirable plant species in the Planting species planning scheme policy.</p>	<p>Y</p>
<p><b>CO10</b> Services and public utilities are protected and any alteration or relocation of services or public utilities meets the standard design specifications of the responsible service authorities.</p>	<p>All existing services and public utilities will be protected or relocated as required.</p>	<p>Y</p>
<p><b>CO11</b> All surface water flows are directed to a lawful point of discharge, if surface water diversion drains are incorporated, they are located at the top or toe of a retaining wall.</p>	<p>Lawful points of discharge will be put in place as required.</p>	<p>Y</p>
<p><b>CO12</b> A retaining wall provides subsoil drainage that is directed to a lawful point of discharge.</p>	<p>All subsoil drainage will be directed to the legal point of discharge.</p>	<p>Y</p>
<p><b>CO13</b> No dust emissions including dust from construction vehicles entering and leaving the site extend beyond the boundary of the site.</p>	<p>Measures will be put in place to reduce all dust emissions from the site.</p>	<p>Y</p>
<p><b>CO14</b> A management plan for control of dust and air emissions is prepared and implemented.</p>	<p>The required management plan will be prepared and implemented on site.</p>	<p>Y</p>
<p><b>CO15</b> <a href="#">Filling or excavation</a> only occurs between 6.30am and 6.30pm Monday to Saturday, excluding public holidays.</p>	<p>All filling will be conducted between the stipulated hours.</p>	<p>Y</p>

<p><b>CO16</b> <a href="#">Filling or excavation</a> does not cause ground-borne vibration beyond the boundary of the site.</p>	<p>Any ground vibration will be limited to the site.</p>	<p>Y</p>
<p><b>CO17</b> Heavy trucks hauling material to and from the site:          (a) occurs for a maximum of 3 weeks;          (b) uses a major road to access the site;          (c) only uses a minor road for the shortest most direct route that has the least amount of environmental nuisance, if there is no major road alternative.</p>	<p>Heavy haulage will not exceed three weeks. Major roads will be used where possible and the use of minor roads only if there is no other alternative.</p>	<p>Y</p>
<p><b>CO18</b> Filling or excavation does not expose the environment or community to contaminated land.</p>	<p>If required precautions will be put in place to prevent any contaminants from being exposed.</p>	<p>Y</p>
<p><b>CO19</b> Landscaping is designed in compliance with the standards in the <a href="#">Landscape design guidelines for water conservation planning scheme policy</a>.</p>	<p>Any Landscaping is designed in compliance with the standards in the Landscape design guidelines for water conservation planning scheme policy.</p>	<p>Y</p>

**Brisbane City Plan  
Filling and Excavation Code  
Performance Outcomes**

Performance Outcomes	Acceptable Outcomes	Proposal	Complies
The following table identifies the assessment criteria for self-assessable and assessable development.			
<p><b>PO1</b> Development for <a href="#">filling or excavation</a> minimises visual impacts from retaining walls and earthworks.</p>	<p><b>AO1</b> Development ensures that the total height of any cut and fill, whether or not retained, does not exceed: (a) 2.5m in a zone in the Industry zones category; (b) 1m in all other zones, or if adjoining a sensitive zone.</p>	<p>The proposed filling may exceed 1m. However, the appropriate retaining measures will be put in place.</p>	<p><b>Y</b></p>
<p><b>PO2</b> Development of a retaining wall proposed as a result of <a href="#">filling or excavation</a>: (a) is designed and constructed to be fit for purpose; (b) does not impact adversely on significant vegetation; (c) is capable of easy maintenance.</p>	<p><b>AO2.1</b> Development of a retaining structure, including footings, surface drainage and subsoil drainage: (a) is wholly contained within the site; (b) if the total height to be retained is greater than 1m, then: (i) the retaining wall at the property boundary is no greater than 1m above the ground level; (ii) all further terracing from the 1m high boundary retaining wall is 1 vertical unit:1 horizontal unit; (iii) the distance between each successive retaining wall (back of lower wall to face of higher wall) is no less than 1m horizontally to incorporate planting areas.</p> <p><b>AO2.2</b> Development of a retaining wall over 1m in height protects significant vegetation on the site and on adjoining land and is designed and constructed in accordance with the structures standards in the <a href="#">Infrastructure design planning scheme policy</a> and</p>	<p>Any retaining walls will designed and constructed in compliance with the structures section of the Infrastructure design planning scheme policy and certified by a Registered Professional Engineer Queensland as required.</p> <p>Any significant vegetation remaining on site or in neighbouring lots will be protected as required.</p>	<p><b>Y</b></p> <p><b>Y</b></p>

	<p>certified by a <a href="#">Registered Professional Engineer Queensland</a>.</p> <p><b>AO2.3</b> Development provides a retaining wall finish that presents to adjoining land that is maintenance free if the setback is less than 750mm from the boundary.</p> <p><b>AO2.4</b> Development for filling only uses clean fill that does not include any construction rubble or debris.</p>	<p>Any retaining walls will be set back the required distance and will be maintenance free for neighbouring properties.</p> <p>All fill transported to site will be free of construction rubble, debris, weed seed or viable parts of plant species listed as an undesirable plant species in the Planting species planning scheme policy.</p>	<p>Y</p> <p>Y</p>
<p><b>PO3</b> Development ensures that a rock anchor is designed and constructed to be fit for purpose.</p>	<p><b>AO3</b> Development ensures that a rock anchor: (a) is constructed in accordance with the standards in the Infrastructure design planning scheme policy; (b) where it extends beyond the property boundary, is supported by a letter of consent from the adjoining land and building owners.</p>	<p>Any required rock anchors will be designed and constructed in accordance with the standards in the Infrastructure design planning scheme policy. Written permission from any affected neighbours will be supplied.</p>	<p>Y</p>
<p><b>PO4</b> Development protects all services and public utilities.</p>	<p><b>AO4</b> Development protects services and public utilities and ensures that any alteration or relocation of services or public utilities meets the standard design specifications of the responsible service authorities.</p>	<p>All existing services and public utilities will be protected or relocated as required.</p>	<p>Y</p>
<p><b>PO5</b> Development provides surface and sub-surface drainage to prevent water seepage, concentration of run-off or ponding of stormwater on adjacent land.</p>	<p><b>AO5</b> Development ensures all flows and subsoil drainage are directed to a lawful point of discharge of a surface water diversion drain, including to the top or toe of a retaining wall in accordance with the stormwater drainage section of the <a href="#">Infrastructure design planning scheme policy</a>.</p>	<p>All subsoil drainage will be directed to the legal point of discharge.</p>	<p>Y</p>
<p><b>PO6</b> Development ensures that the design and construction of all open drainage works is undertaken in accordance with natural</p>	<p><b>AO6</b> No acceptable outcome is prescribed.</p>	<p>Not Applicable</p>	<p>N/A</p>

<p>channel design principles, being the development of a stormwater conveyance system for major flows, by using a vegetated open channel or drain that approximates the features and functions of a natural waterway to enhance or improve riparian values of those stormwater conveyance systems.</p>			
<p><b>PO7</b> Development for filling or excavation: (a) does not degrade water quality or adversely affect environmental values in receiving waters; (b) ensures site sediment and erosion control standards are best practice.</p>	<p><b>A07.1</b> Development for filling or excavation provides water quality treatment that complies with the stormwater drainage section of the <a href="#">Infrastructure design planning scheme policy</a>.</p> <p><b>A07.2</b> Development provides erosion and sediment control standards that are in accordance with the stormwater drainage section of the <a href="#">Infrastructure design planning scheme policy</a>.</p>	<p>If required water quality treatment measures will be put in place to comply with the stormwater drainage section of the Infrastructure design planning scheme policy</p> <p>Erosion Sediment Controls will be implemented on site in accordance with the stormwater drainage section of the Infrastructure design planning scheme policy.</p>	<p><b>Y</b></p> <p><b>Y</b></p>
<p><b>PO8</b> Development for <a href="#">filling or excavation</a> is conducted such that adverse impacts at a sensitive use due to noise and dust are prevented or minimised. Note—A noise and dust impact management plan prepared in accordance with the <a href="#">Management plans planning scheme policy</a> can assist in demonstrating achievement of this performance outcome.</p>	<p><b>A08.1</b> Development ensures that no dust emissions extend beyond the boundary of the site, including dust from construction vehicles entering and leaving the site.</p> <p><b>A08.2</b> Development for <a href="#">filling or excavation</a> activity only occurs between the hours of 6:30am and 6:30pm Monday to Saturday, excluding public holidays.</p>	<p>Measures will be put in place to reduce all dust emissions from the site.</p> <p>All filling will be conducted between the stipulated hours.</p>	<p><b>Y</b></p> <p><b>Y</b></p>
<p><b>PO9</b> Development ensures that vibration generated by the <a href="#">filling or excavation</a> operation does not exceed the vibration</p>	<p><b>A09</b> Development involving filling or excavation does not cause a ground-borne vibration beyond the boundary of the site.</p>	<p>Any ground vibration will be limited to the site.</p>	<p><b>Y</b></p>

<p>criteria in <a href="#">Table 9.4.3.3.D</a>, <a href="#">Table 9.4.3.3.E</a>, <a href="#">Table 9.4.3.3.F</a> and <a href="#">Table 9.4.3.3.G</a>.</p> <p>Note—A noise management report prepared in accordance with the Noise impact assessment planning scheme policy can assist in demonstrating achievement of this performance outcome.</p>			
<p><b>PO10</b> Development ensures that heavy trucks hauling material to and from the site do not affect the amenity of established areas and limits environmental nuisance impact on adjacent land.</p>	<p><b>AO10</b> Development ensures that heavy trucks hauling material to and from the site: (a) occur for a maximum of 3 weeks; (b) use a major road to access the site; (c) only use a minor road for the shortest-most-direct route that has the least amount of environmental nuisance if there is no major road alternative.</p>	<p>Heavy haulage will not exceed three weeks. Major roads will be used where possible and the use of minor roads only if there is no other alternative.</p>	<p><b>Y</b></p>
<p><b>PO11</b> Development for filling or excavation protects the environment and community health and wellbeing from exposure to contaminated land and contaminated material.</p>	<p><b>AO11</b> Development does not involve: (a) excavation on land previously occupied by a notifiable activity or on land listed on the <a href="#">Environmental Management Register</a> or the <a href="#">Contaminated Land Register</a>; (b) filling with material containing a contaminant.</p>	<p>Measures will be put in place so that any excavation will protect the environment and community health and wellbeing from exposure to contaminated land and contaminated material. No filling material will have any contaminants.</p>	<p><b>Y</b></p>
<p><b>PO12</b> Development provides for: (a) landscaping for water conservation purposes; (b) water sensitive urban design measures which are employed within the landscape design to maximise stormwater use and to reduce any adverse impacts on the landscape;</p>	<p><b>AO12.1</b> Development provides landscaping which is designed using the standards in the Landscape design guidelines for water conservation planning scheme policy.</p>	<p>Any Landscaping is designed in compliance with the standards in the Landscape design guidelines for water conservation planning scheme policy.</p>	<p><b>Y</b></p>

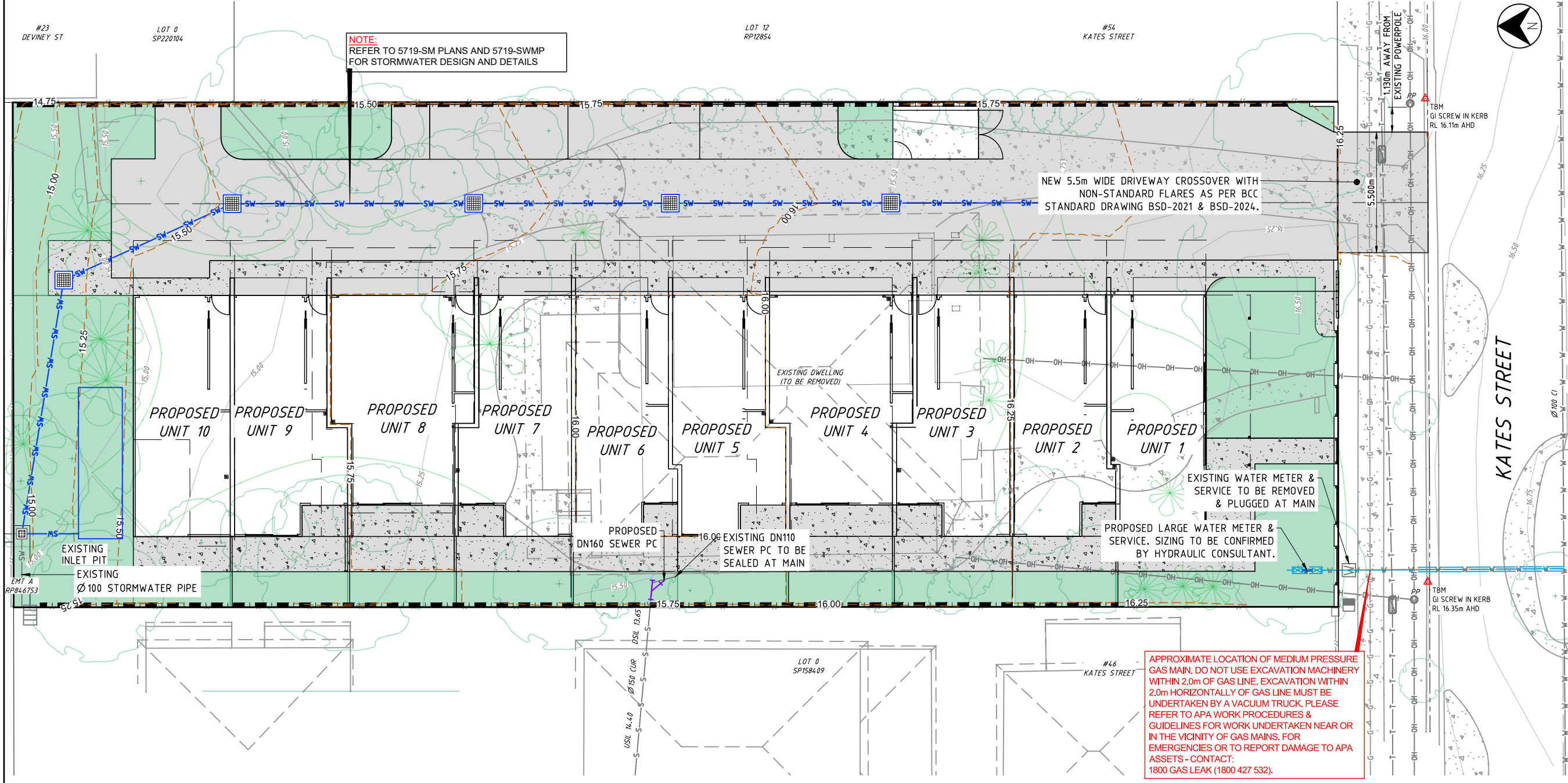
(c) stormwater harvesting to be maximised and any adverse impacts of stormwater minimised.			
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# APPENDIX 1. CONCEPT DESIGN PLANS

**WARNING!**  
SUBSURFACE UTILITY INVESTIGATION SHOULD BE UNDERTAKEN TO DETERMINE DETAILS OF WATER, SEWER & OTHER SERVICES. ACCURACY OF FINAL DESIGN WILL DEPEND ON THIS BEING UNDERTAKEN.

DO NOT SCALE. CONFIRM ALL DIMENSIONS ON SITE. Copyright

REV.	AMENDMENT	BY	DATE
PA	PRELIMINARY ISSUE	RM	20.01.26



**NOTE:**  
REFER TO 5719-SM PLANS AND 5719-SWMP FOR STORMWATER DESIGN AND DETAILS

NEW 5.5m WIDE DRIVEWAY CROSSOVER WITH NON-STANDARD FLARES AS PER BCC STANDARD DRAWING BSD-2021 & BSD-2024.

APPROXIMATE LOCATION OF MEDIUM PRESSURE GAS MAIN. DO NOT USE EXCAVATION MACHINERY WITHIN 2.0m OF GAS LINE. EXCAVATION WITHIN 2.0m HORIZONTALLY OF GAS LINE MUST BE UNDERTAKEN BY A VACUUM TRUCK. PLEASE REFER TO APA WORK PROCEDURES & GUIDELINES FOR WORK UNDERTAKEN NEAR OR IN THE VICINITY OF GAS MAINS. FOR EMERGENCIES OR TO REPORT DAMAGE TO APA ASSETS - CONTACT: 1800 GAS LEAK (1800 427 532).

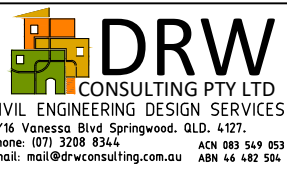
**LEGEND**

- EXISTING SEWER LINE & MANHOLE
- EXISTING TELSTRA / OPTIC FIBRE & PIT
- EXISTING WATER
- EXISTING STORMWATER
- EXISTING GAS
- EXISTING OH ELECTRICITY
- EXISTING UIG ELECTRICITY
- EXISTING KERB INVERT
- EXISTING CONTOURS
- EXISTING CONCRETE
- EXISTING BUILDING LINE
- EXISTING BUILDING ROOF
- EXISTING RETAINING WALL
- FIRE HYDRANT
- PROPOSED SWALE DRAIN
- PROPOSED DRIVEWAY
- PROPOSED STORMWATER
- PROPOSED SEWER
- DESIGN CONTOURS
- PROPOSED WATER SERVICE
- PROPOSED WATER METER
- PROPOSED RETAINING WALL
- PROPOSED EASEMENT
- PROPOSED TOP OF BATTER
- PROPOSED BOTTOM OF BATTER
- PROPOSED SPOT LEVEL
- DESIGN BUILDING GROUND
- DESIGN WATER CONDUIT
- DESIGN ELECTRICAL CONDUIT
- DESIGN TELECOM CONDUIT
- DESIGN HOUSE DRAIN SEWER
- DESIGN ROOF WATER
- DESIGN EDGE OF ROAD KERB

**SURVEY INFORMATION**  
SURVEYOR:  
**AJS SURVEYS**  
DRAWING No.:  
**25353-D1-A**  
VERTICAL DATUM:  
**AHD**  
REAL PROPERTY DESCRIPTION:  
**LOT 2 ON RP82062**  
SERVICES ALSO PLOTTED FROM:



THIS DOCUMENT IS TO BE READ IN CONJUNCTION WITH BEFORE YOU DIG DOCUMENTATION & LOCATION OF SERVICES CONFIRMED PRIOR TO ON-SITE CONSTRUCTION OR EXCAVATION COMMENCES.



**DRW CONSULTING PTY LTD**  
CIVIL ENGINEERING DESIGN SERVICES  
2/16 Vanessa Blvd Springwood, QLD, 4127.  
Phone: (07) 3208 8344 ACN 083 549 053  
Email: mail@drwconsulting.com.au ABN 46 482 504 266

PROJECT SITE:  
**50 KATES STREET, MORNINGSIDE SERVICING LAYOUT**

Scale	Drawn	Date
AS SHOWN	NS	20.11.25

Drawing No. **5719-SK01** Rev. **PA**

ALL EXISTING PROPERTY CONNECTIONS THAT ARE TO BE REUSED ARE TO HAVE CCTV FOOTAGE TAKEN TO ENSURE THEY ARE IN GOOD WORKING ORDER

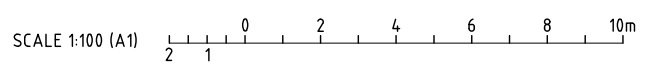
CLEAR, UNOBSTRUCTED, TWENTY FOUR (24) HOUR ACCESS FROM THE STREET FRONTAGE IS TO BE MAINTAINED TO ALL SEWERAGE MAINTENANCE HOLES.

NEW & EXISTING WATER METERS TO BE INSTALLED A MINIMUM OF 1.1m CLEAR OF ANY PROPOSED ELECTRICAL PILLAR. LOCATION OF FUTURE ELECTRICAL CONNECTIONS UNKNOWN AT THIS TIME

THIS DESIGN HAS BEEN PREPARED BASED ON SERVICE AUTHORITY AS CONSTRUCTED INFORMATION. NO POT HOLING HAS BEEN UNDERTAKEN TO VERIFY EXISTING SERVICE LOCATIONS & DEPTHS. IT IS THE CONTRACTORS RESPONSIBILITY TO UNDERTAKE POT HOLING TO VERIFY THE DESIGN.

**NOT FOR CONSTRUCTION**

**ISSUED FOR INFORMATION**



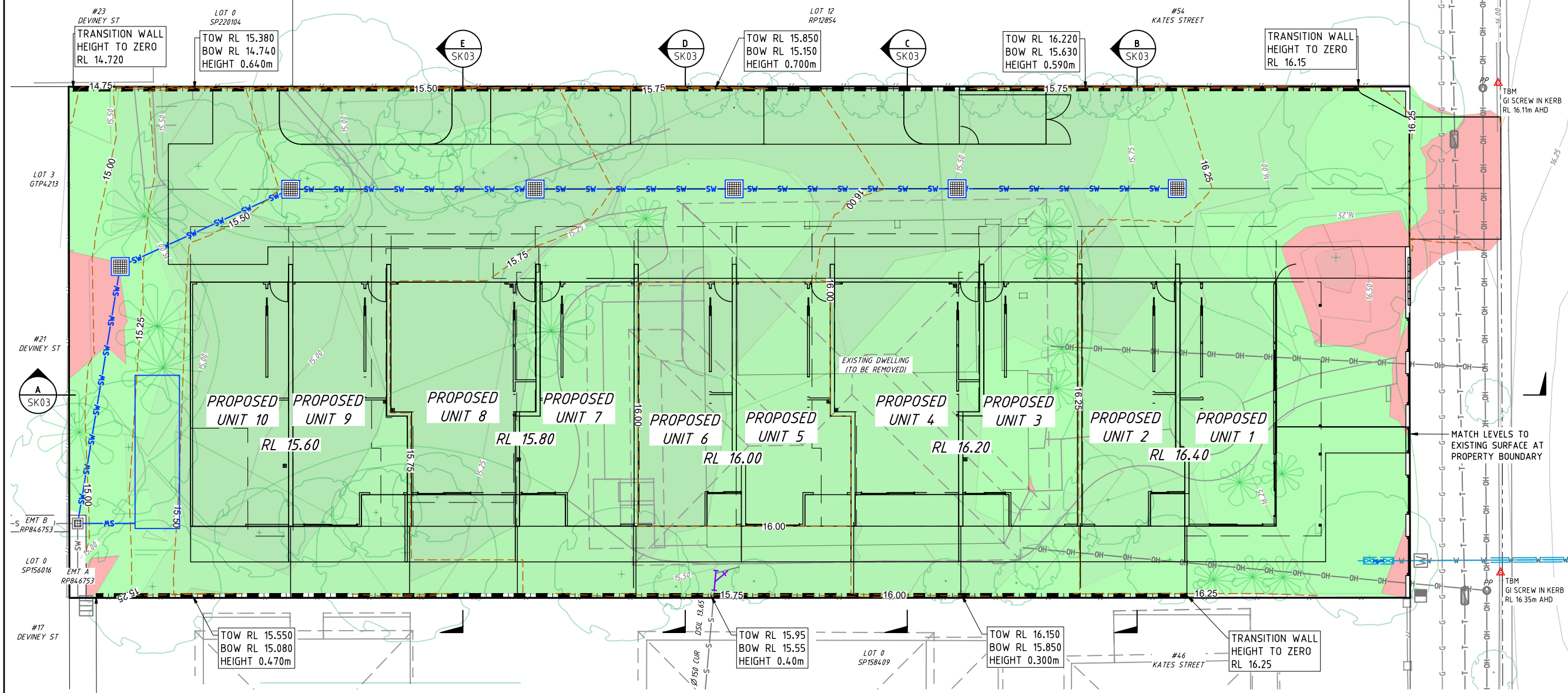
**BCC DS RECEIVED**  
 11/02/2026  
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REV.	AMENDMENT	BY	DATE
PA	PRELIMINARY ISSUE	RM	20.01.26

**LEGEND**

- EXISTING SEWER LINE & MANHOLE
- EXISTING TELSTRA / OPTIC FIBRE & PIT
- EXISTING WATER
- EXISTING STORMWATER
- EXISTING GAS
- EXISTING OH ELECTRICITY
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- DESIGN BUILDING GROUND
- DESIGN WATER CONDUIT
- DESIGN ELECTRICAL CONDUIT
- DESIGN TELECOM CONDUIT
- DESIGN HOUSE DRAIN SEWER
- DESIGN ROOF WATER
- DESIGN EDGE OF ROAD KERB



**CONCEPT EARTHWORKS LAYOUT**  
 SCALE 1:100

ALL EXISTING PROPERTY CONNECTIONS THAT ARE TO BE REUSED ARE TO HAVE CCTV FOOTAGE TAKEN TO ENSURE THEY ARE IN GOOD WORKING ORDER

CLEAR, UNOBSTRUCTED, TWENTY FOUR (24) HOUR ACCESS FROM THE STREET FRONTAGE IS TO BE MAINTAINED TO ALL SEWERAGE MAINTENANCE HOLES.

NEW & EXISTING WATER METERS TO BE INSTALLED A MINIMUM OF 1.1m CLEAR OF ANY PROPOSED ELECTRICAL PILLAR. LOCATION OF FUTURE ELECTRICAL CONNECTIONS UNKNOWN AT THIS TIME

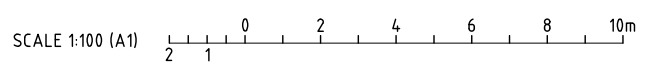
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**CUT & FILL HATCHES**

Range	Direction
>3.000	FILL
2.000 - 2.999	FILL
1.500 - 1.999	FILL
1.000 - 1.499	FILL
0.500 - 0.999	FILL
0.250 - 0.499	FILL
0.000 - 0.249	FILL
-0.249 - 0.000	CUT
-0.499 - -0.250	CUT
-0.999 - -0.500	CUT
-1.499 - -1.000	CUT
-1.999 - -1.500	CUT
-2.999 - -2.000	CUT
<-3.000	CUT

NOT FOR CONSTRUCTION

ISSUED FOR INFORMATION



**SURVEY INFORMATION**  
 SURVEYOR:  
**AJS SURVEYS**

DRAWING No.:  
**25353-D1-A**

VERTICAL DATUM:  
**AHD**

REAL PROPERTY DESCRIPTION:  
**LOT 2**  
**ON RP82062**  
 SERVICES ALSO PLOTTED FROM:



THIS DOCUMENT IS TO BE READ IN CONJUNCTION WITH BEFORE YOU DIG DOCUMENTATION & LOCATION OF SERVICES CONFIRMED PRIOR TO ON-SITE CONSTRUCTION OR EXCAVATION COMMENCES.

**DRW**  
 CONSULTING PTY LTD  
 CIVIL ENGINEERING DESIGN SERVICES  
 2/16 Vanessa Blvd Springwood, QLD, 4127.  
 Phone: (07) 3208 8344 ACN 083 549 053  
 Email: mail@drwconsulting.com.au ABN 46 482 504 266

PROJECT SITE:  
**50 KATES STREET, MORNINGSIDE**  
**CONCEPT EARTHWORKS**

Scale	Drawn	Date
AS SHOWN	NS	20.11.25

Drawing No.	Rev.
5719-SK02	PA

BCC DS  
RECEIVED

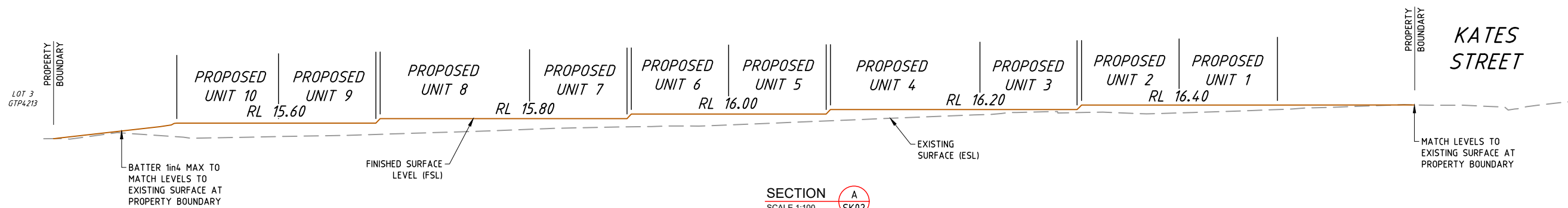
11/02/2026

APPLICATION REF

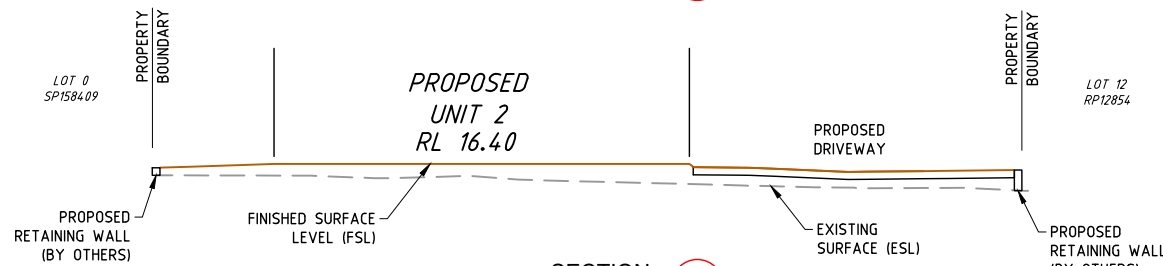
A006958895

DO NOT SCALE.  
CONFIRM ALL DIMENSIONS ON SITE.

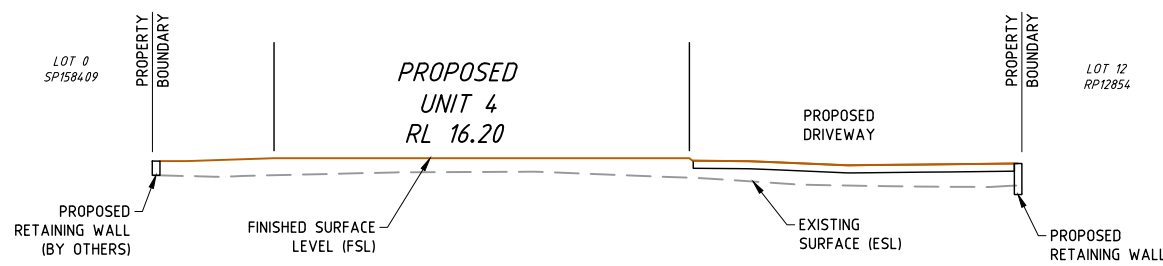
REV.	AMENDMENT	BY	DATE
PA	PRELIMINARY ISSUE	RM	20.01.26



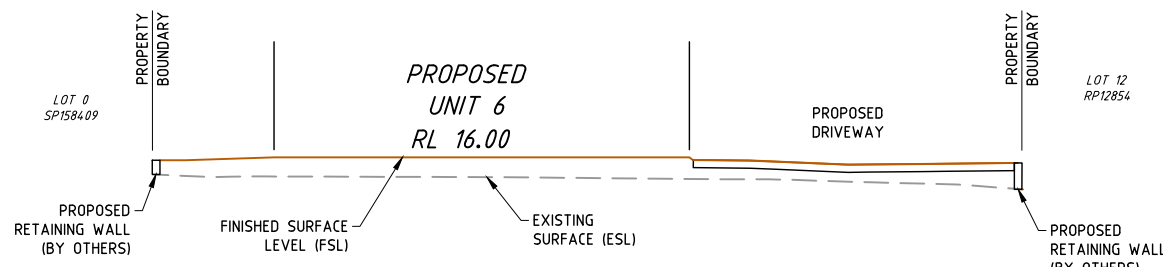
SECTION A  
SCALE 1:100  
SK02



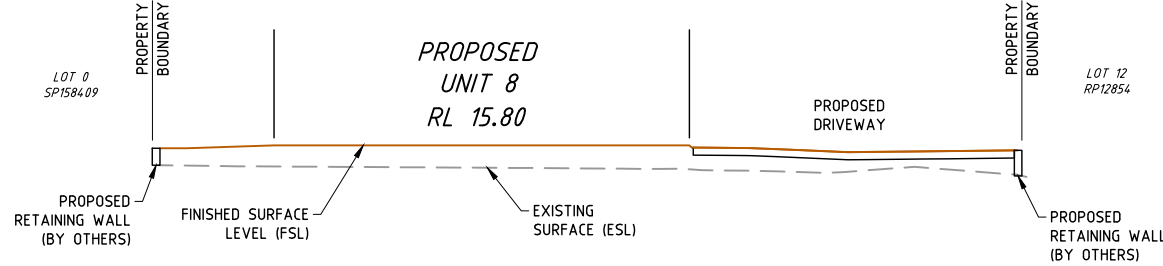
SECTION B  
SCALE 1:100  
SK02



SECTION C  
SCALE 1:100  
SK02



SECTION D  
SCALE 1:100  
SK02



SECTION E  
SCALE 1:100  
SK02

ALL EXISTING PROPERTY CONNECTIONS THAT ARE TO BE REUSED ARE TO HAVE CCTV FOOTAGE TAKEN TO ENSURE THEY ARE IN GOOD WORKING ORDER

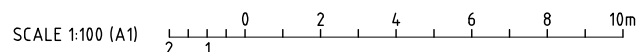
CLEAR, UNOBSTRUCTED, TWENTY FOUR (24) HOUR ACCESS FROM THE STREET FRONTAGE IS TO BE MAINTAINED TO ALL SEWERAGE MAINTENANCE HOLES.

NEW & EXISTING WATER METERS TO BE INSTALLED A MINIMUM OF 1.1m CLEAR OF ANY PROPOSED ELECTRICAL PILLAR. LOCATION OF FUTURE ELECTRICAL CONNECTIONS UNKNOWN AT THIS TIME

THIS DESIGN HAS BEEN PREPARED BASED ON SERVICE AUTHORITY AS CONSTRUCTED INFORMATION. NO POT HOLING HAS BEEN UNDERTAKEN TO VERIFY EXISTING SERVICE LOCATIONS & DEPTHS. IT IS THE CONTRACTORS RESPONSIBILITY TO UNDERTAKE POT HOLING TO VERIFY THE DESIGN.

**NOT FOR CONSTRUCTION**

**ISSUED FOR INFORMATION**



**SURVEY INFORMATION**

SURVEYOR:  
**AJS SURVEYS**  
  
DRAWING No.:  
**25353-D1-A**  
VERTICAL DATUM:  
**AHD**  
REAL PROPERTY DESCRIPTION:  
**LOT 2  
ON RP82062**  
SERVICES ALSO PLOTTED FROM:



THIS DOCUMENT IS TO BE READ IN CONJUNCTION WITH BEFORE YOU DIG DOCUMENTATION & LOCATION OF SERVICES CONFIRMED PRIOR TO ON-SITE CONSTRUCTION OR EXCAVATION COMMENCES.

**DRW**  
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Phone: (07) 3208 8344 ACN 083 569 053  
Email: mail@drwconsulting.com.au ABN 46 482 504 266

PROJECT SITE:  
**50 KATES STREET,  
MORNINGSIDE  
EARTHWORKS SECTIONS**

Scale	Drawn	Date
AS SHOWN	NS	20.11.25
Drawing No.	5719-SK03	Rev.
		PA

# APPENDIX 2. EROSION HAZARD ASSESSMENT



# Erosion Hazard Assessment - June 2014

Brisbane City Council (BCC), *Erosion Hazard Assessment* form must be read in conjunction with the *Erosion Hazard Assessment- Supporting Technical Notes* (June 2014 or later version) for explanatory terms and Certification information.

## What is an Erosion Hazard Assessment?

Soil erosion and sediment from urban development, particularly during construction activities, is a significant source of sediment pollution in Brisbane's waterways. The Erosion Hazard Assessment determines whether the risk of soil erosion and sediment pollution to the environment is 'low', 'medium' or 'high'.

## When is the EHA required?

An *Erosion Hazard Assessment* form must be completed and lodged with BCC for any Development Application (ie MCU or ROL) that will result in soil disturbance OR Operational Works or Compliance Assessment Application for 'Filling' or Excavation.

**Failure to submit this form during lodgement of an application may result in assessment delays or refusal of the application.**

## Privacy Statement

The personal information collected on this form will be used by Brisbane City Council for the purposes of fulfilling your request and undertaking associated Council functions and services. Your personal information will not be disclosed to any third party without your consent, unless this is required or permitted by law.

## Assessment Details

1 Please turn over and complete the erosion hazard assessment.

2 Based on the erosion hazard assessment overleaf, is the site:

A 'low' risk site

*Best practice erosion and sediment control (ESC) must be implemented but no erosion and sediment control plans need to be submitted with the development application. Factsheets outlining best practice ESC can be found at <http://www.waterbydesign.com.au/factsheets>*

A 'medium' risk site

*If the development is approved, the applicant will need to engage a Registered Professional Engineer (RPEQ) or Certified Professional in Erosion and Sediment Control (CPESC) to prepare an ESC Program and Plan and supporting documentation — in accordance with the requirements of the Infrastructure Design Planning Scheme Policy.*

A 'high' risk site

*If the development is approved, the applicant will need to engage a RPEQ and CPESC to prepare an ESC Program and Plan and supporting documentation — in accordance with the requirements of the Infrastructure Design Planning Scheme Policy. The plans and program will need to be certified by a CPESC.*

## 3 Site Information and Certification

Application number (if known)

Site address

50 KATES STREET, MORNINGSIDE

Postcode

I certify that:

- I have made all relevant enquiries and am satisfied no matters of significance have been withheld from the assessment manager.
- I am a person with suitable qualifications and/or experience in erosion and sediment control.
- The Erosion Hazard Assessment was completed in accordance with the Erosion Hazard Assessment Supporting Technical Notes and the BCC Infrastructure Design Planning Scheme Policy.
- The Erosion Hazard Assessment accurately reflects the site's overall risk of soil erosion and sediment pollution to the environment.
- I acknowledge and accept that the BCC, as assessment manager, relies, in good faith, on this certification as part of its development assessment process and the provision of false or misleading information to the BCC constitutes an offence for which BCC may take punitive steps/ action against me/ enforcement action against me.

Certified by *Print name*

Dave Weston

Certifier's signature

Date

28/ 1 /2026

**Table 1: Low Risk Test**

		Yes	No
1.1	is the area of land disturbance > 1000 m <sup>2</sup>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
1.2	does any land disturbance occur in a BCC mapped waterway corridor	<input type="checkbox"/>	<input checked="" type="checkbox"/>
1.3	is there any slope on site (longer than three metres in length) before, during or after construction that is steeper than 5%	<input checked="" type="checkbox"/>	<input type="checkbox"/>
1.4	does any land disturbance occur below 5 m AHD	<input type="checkbox"/>	<input checked="" type="checkbox"/>
1.5	does development involve endorsement of a staging plan	<input type="checkbox"/>	<input checked="" type="checkbox"/>
1.6	is there an upstream catchment passing through the site > 1 hectare	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Have you answered 'yes' to any of the questions in Table 1?

Yes	No
<input checked="" type="checkbox"/>	<input type="checkbox"/>

If 'Yes' then proceed to Table 2

If 'No' then site is **low risk** with respect to erosion and sediment control

**Table 2: Medium Risk Test**

		Yes	No
2.1	is the area of land disturbance > 1 hectare	<input type="checkbox"/>	<input checked="" type="checkbox"/>

If 'Yes' then proceed to Table 3

If 'No' then site is **medium risk** with respect to erosion and sediment control

**Table 3: High Risk Test**

3.1	is there an upstream catchment passing through the site > 1 hectare	<input type="checkbox"/>	<input type="checkbox"/>
3.2	does any land disturbance occurs in a BCC mapped waterway corridor	<input type="checkbox"/>	<input type="checkbox"/>
3.3	is there any slope on site (longer than three metres in length) before, during or after construction that is steeper than 15%	<input type="checkbox"/>	<input type="checkbox"/>

Have you answered 'yes' to any of the questions in Table 3?

Yes	No
<input type="checkbox"/>	<input type="checkbox"/>

If 'Yes' then site is **high risk** with respect to erosion and sediment control

If 'No' then site is **medium risk** with respect to erosion and sediment control

# APPENDIX 3. FLOODWISE PROPERTY REPORT

# FloodWise Property Report

50 KATES ST, MORNINGSIDE 4170  
Lot 2 on RP82062



Dedicated to a better Brisbane


## THE PURPOSE OF THIS REPORT IS FOR BUILDING AND DEVELOPMENT

Brisbane City Council's FloodWise Property Report provides technical flood planning information including estimated flood levels, habitable floor level requirements and more. This report uses the adopted flood planning information in Brisbane City Plan 2014, that guides how land in Brisbane is used and developed for the future. Find out more about [planning and building](#). To understand how to be resilient and prepare for floods, visit Council's [Be Prepared](#) webpage. Find more information about [how to read a FloodWise Property Report](#).

### This property has no flood levels

Brisbane City Council has not assigned flood level information for this property however it may be affected by one or more flood or property development flags. Please refer to the Flood Planning and Development Information below for details. The property may have 0.2% AEP flood level which will appear on the Flood Planning Information table if applicable. For professional advice or detailed assessment of a property contact a Registered Professional Engineer of Queensland.

Visit the [Be Prepared](#) page to find more information on how to prepare your home or business for potential flooding.

 **Combined** 1% AEP for river, creek and storm tide flood extent (if applicable) from the adopted Brisbane City Plan 2014. Read more about [Brisbane City Plan 2014](#).



Brisbane City Council | Includes material ©The State of Queensland, all rights reserved, 2019. | ©Brisbane City C... Powered by Esri

# Are you resilient and ready for flood?

- Sign up to the Brisbane Severe Weather Alert at [brisbane.qld.gov.au/beprepared](https://brisbane.qld.gov.au/beprepared)
- Visit [bom.gov.au](https://bom.gov.au) for the latest weather updates.
- Have an evacuation plan, emergency kit and important phone numbers ready.
- Observe where water flows from and to during heavy rain.
- Consider how flood-resilient building techniques will have you home faster and with less damage.

Life threatening emergencies  
**000** Police/fire/ambulance  
(mobiles **000** and **112**)

State Emergency Service (SES) **132 500**  
Energex **13 19 62**  
Brisbane City Council **3403 8888**

## Technical Summary

This section of the FloodWise Property Report contains more detailed flood information for this property so **surveyors, builders, certifiers, architects, and engineers can plan and build** in accordance with Council's planning scheme.

Find more information about [planning and building](#) in Brisbane or talk to a Development Services Planning Information Officer via Council's Contact Centre on (07) 3403 8888.

## Flood Planning and Development Information

This section of the FloodWise Property Report contains information about Council's planning scheme overlays. Overlays identify areas within the planning scheme that reflect distinct themes that may include constrained land and/or areas sensitive to the effects of development.

### Flood overlay code

The Flood overlay code of Council's planning scheme uses the following information to provide guidelines when developing properties. The table below summarises the flood planning areas (FPAs) that apply to this property. Development guidelines for the FPAs are explained in [Council's planning scheme](#).

Flood planning areas (FPA)		
River	Creek / waterway	Overland flow
		Not Applicable

To find more information about Council's flood planning areas (FPAs) for Brisbane River and Creek/waterway flooding to guide future building and development in flood prone areas, please review [Council's Flood Planning Provisions](#).

### Coastal hazard overlay code

The Coastal hazard overlay code of Council's planning scheme uses the following information to provide guidelines when conducting new developments. The table below summarises the coastal hazard categories that apply to this property. Development guidelines for the following Coastal hazard overlay sub-categories are explained in Council's [planning scheme](#).

Coastal hazard overlay sub-categories
There are currently no Coastal hazard overlay sub-categories that apply to this property.

Note: Where land is identified within one or more flood planning areas on the Flood overlay or is identified within one of the Storm tide inundation area sub-categories on the Coastal hazard overlay, the assessment criteria that provides the highest level of protection from any source of flooding applies.

### Property development flags

**Large allotment** - This property is either a Large Allotment of over 1000 square metres or is located within a Large Allotment. Flood levels may vary significantly across allotments of this size. Further investigations may be warranted in determining the variation in flood levels and the minimum habitable floor level across the site.

For more information or advice, please consult a Registered Professional Engineer of Queensland (RPEQ).

## Useful Flood Information Definitions

**Australian Height Datum (AHD)** - The reference level for defining ground levels in Australia. The level of 0.0m AHD is approximately mean sea level.

**Annual Exceedance Probability (AEP)** - The probability of a flood event of a given size occurring in any one year, usually expressed as a percentage annual chance.

- **0.2% AEP** - A flood event of this size is considered rare but may still occur. A flood of size or larger has a 1 in 500 chance or a 0.2% probability of occurring in any year.
- **1% AEP** - A flood of this size or larger has a 1 in 100 chance or a 1% probability of occurring in any year.
- **2% AEP** - A flood of this size or larger has a 1 in 50 chance or a 2% probability of occurring in any year.
- **5% AEP** - A flood of this size or larger has a 1 in 20 chance or a 5% probability of occurring in any year.
- **20% AEP** - A flood of this size or larger has a 1 in 5 chance or a 20% probability of occurring in any year.

### Data quality

- **Data Quality Code A** - Level data based on recent surveyor report or approved as-constructed drawings.
- **Data Quality Code B** - Level data based on ground-based mobile survey or similar.
- **Data Quality Code C** - Level data derived from Airborne Laser Scanning or LiDAR information.

**Defined Flood Level (DFL)** - The DFL is used for commercial and industrial development. The Defined flood level (DFL) for Brisbane River flooding is a level of 3.7m AHD at the Brisbane City Gauge based on a flow of 6,800 m/s. DFL is only applicable for non-residential uses affected by Brisbane River flooding.

**Flood planning area (FPA)** - Council has developed five Flood planning areas (FPAs) as part of Brisbane City Plan 2014 Flood overlay mapping for Brisbane River, Creek/waterway flooding and Overland flow to guide future building and development in flood prone areas. Storm tide flooding is mapped separately. The FPAs are designed to recognise the flood hazard for different flooding types. Flood hazard is a combination of frequency of flooding, the flood depth, and the speed at which the water is travelling. [Find more information here.](#)

**Maximum and minimum ground level** - Highest and lowest ground levels on the property based on available ground level information. A Registered Surveyor can confirm exact ground levels.

**Minimum habitable floor level (dwelling house)** - The minimum level in metres AHD at which habitable areas of development (generally including bedrooms, living rooms, kitchen, study, family, and rumpus rooms) must be constructed as required by the Brisbane City Plan 2014.

**Indicative existing floor level** - The approximate level in metres AHD of the lowest habitable floor in the existing building (excluding apartments). The data is sourced from a range of sources with varying accuracy levels.

**Property** - A property will contain 1 or more lots. The multiple lot warning is shown if you have selected a property that contains multiple lots.

**Residential flood level (RFL)** - This flood level for the Brisbane River equates to the 1% annual exceedance probability (AEP) flood level.

To learn more, visit [Brisbane City Council's Flood Information Hub](#)

## Brisbane City Council's Online Flood Tools

Council provides several online flood tools:

- to guide planning and development
- to help residents and businesses understand their flood risk and prepare for flooding.

Council's online flood tools for planning and development purposes include:

- **FloodWise Property Report**
- **Flood Overlay Code**

For more information on Council's planning scheme and online flood tools for planning and development:

- phone (07) 3403 8888 and ask to talk to a Development Services Planning Information Officer

- visit [brisbane.qld.gov.au/planning-building](https://brisbane.qld.gov.au/planning-building)

Council's Planning Scheme - The Brisbane City Plan 2014 (planning scheme) has been prepared in accordance with the Sustainable Planning Act as a framework for managing development in a way that advances the purpose of the Act. In seeking to achieve this purpose, the planning scheme sets out the Council's intention for future development in the planning scheme area, over the next 20 years.

### Disclaimer

1. Defined flood levels and residential flood levels, minimum habitable floor levels and indicative existing floor levels are determined from the best available information to Council at the date of issue. These levels, for a particular property, may change if more detailed information becomes available or changes are made in the method of calculating levels.
2. Council makes no warranty or representation regarding the accuracy or completeness of a FloodWise Property Report. Council disdaims any responsibility or liability in relation to the use or reliance by any person on a FloodWise Property Report.



### Planning to build or renovate?

For information, guidelines, tools and resources to help you track, plan or apply for your development visit [brisbane.qld.gov.au/planning-building](https://brisbane.qld.gov.au/planning-building)

You can also find the Brisbane City Plan 2014 and Neighbourhood Plans as well as other information and training videos to help, with your building and development plans.