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APPLICATION REF
A006826022



Traffic Impact Assessment

2-6 Ashmore Street
Everton Park
Short Term Accommodation and Multiple Dwelling Development

REPORT

PREPARED FOR:



25 May 2026

Reference: 80669

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Document Status



Rev No.	Author(s)	Reviewed / Approved		Description	Date
		Name	Signature		
1	Harry Dempsey RPEQ 27009	Marko Mrsic		Original issue	16/12/2025
2	Harry Dempsey RPEQ 27009	Marko Mrsic		Council Comments	25/05/2026

Table of Contents

1.	Introduction	1
2.	Proposed Development	2
3.	Existing Traffic Conditions	3
3.1	Subject Site	3
3.2	Road Network	3
3.3	Public and Active Transport	4
4.	Development Traffic Assessment	6
4.1	Site Access	6
4.1.1	Site Access Design	6
4.1.2	Sight Distance	7
4.2	Parking	8
4.2.1	Parking Supply	8
4.2.2	Parking Design	8
4.3	Servicing	10
4.4	Active Transport	11
5.	Traffic Impacts	12
5.1	Estimated Traffic Generation	12
5.2	Road Network Impact	13
6.	Summary and Recommendation	14
6.1	Summary	14
6.2	Recommendation	14

Table Index

Table 1:	Parking Space Dimensions	9
Table 2:	Trip Generation Rates (TfNSW Technical Guidance)	12
Table 3:	Estimated Development Traffic Generation	12

Figure Index

Figure 1:	Site Locality	1
Figure 2:	Proposed Site Plan	2

Figure 3:	Ashmore Street	3
Figure 4:	Newhaven Street	4
Figure 5:	South Pine Road	4
Figure 6:	Public Transport	5
Figure 7:	Type B2 Vehicle Crossing	6
Figure 8:	BSD-2025: Vehicle Crossing Grades	7
Figure 9:	Access Sight Distance – Newhaven Street	8
Figure 10:	Vehicle Swept Path – B85	10
Figure 11:	Vehicle Swept Path - RCV	11

Appendices

- A Vehicle Swept Path Analysis

1. Introduction



ITE Traffic was engaged to prepare this Traffic Impact Assessment for a proposed multiple dwelling development, which addresses all relevant traffic and transport requirements outlined in the Brisbane City Council (BCC) City Plan.

The proposed development is located at 2-6 Ashmore Street, Everton Park, as shown in Figure 1.

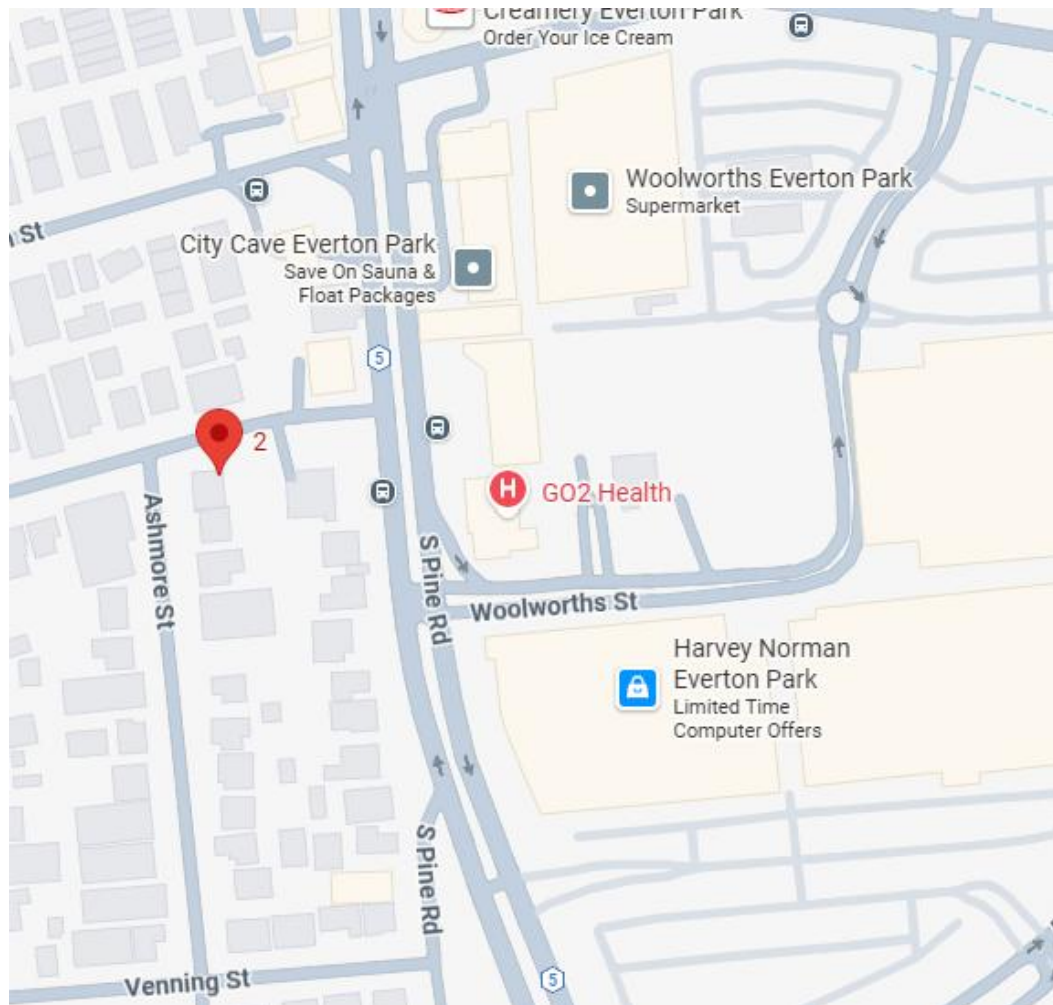


Figure 1: Site Locality

2. Proposed Development

The proposed development is for a short term accommodation and multiple dwelling that consists of 51 x 1-bedroom units with a basement car park, as shown in Figure 2.

It is noted that most units are for short term accommodation except for 3 ground floor units that are allocated as multiple residential dwellings.

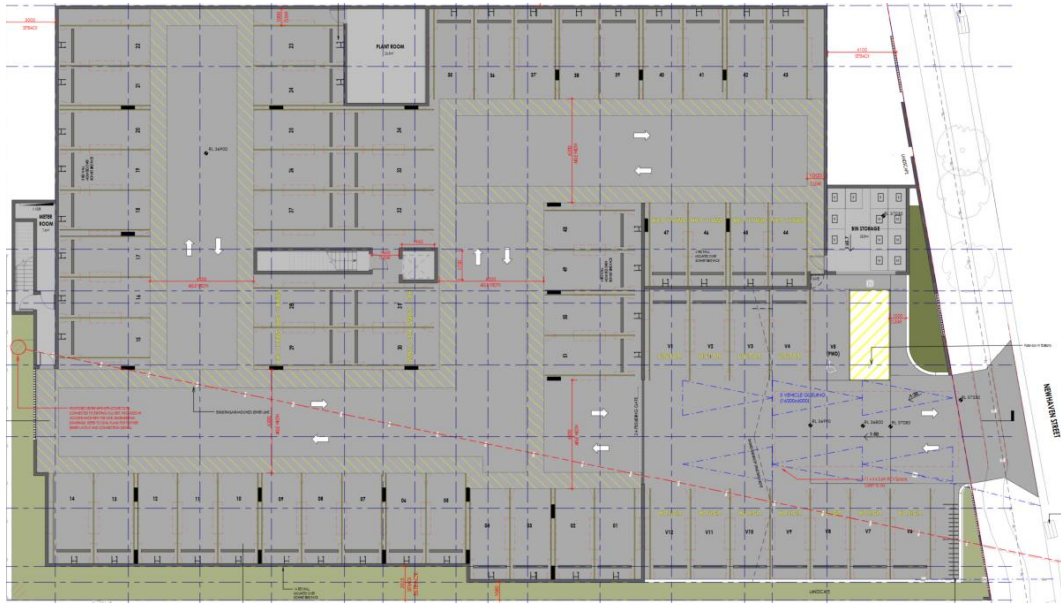


Figure 2: Proposed Site Plan

3. Existing Traffic Conditions

3.1 Subject Site

The site is located along Ashmore Street, across 3 existing residential lots.

3.2 Road Network

The road network surrounding the site is managed by BCC with details as follows:

▶ **Ashmore Street:**

- Council controlled neighbourhood road.
- 2-way road with informal parking both sides.
- Intermittent pedestrian footpath along both sides of the road.
- 40km/h speed limit area.



Figure 3: Ashmore Street

▶ **Newhaven Street:**

- Council controlled neighbourhood road.
- 2-way road with informal parking both sides.
- Pedestrian footpath along both sides of the road.
- 40km/h speed limit area.



Figure 4: Newhaven Street

▶ **South Pine Road:**

- Council controlled arterial road.
- 4 lane dual carriageway with no parking either side.
- Pedestrian footpath along both sides of the road.
- 60km/h posted speed limit.



Figure 5: South Pine Road

3.3 Public and Active Transport

The surrounding pedestrian and cyclist network is currently accessed via the existing footpaths along Newhaven Street. Cyclists can also be accommodated on street, travelling within the available traffic lanes.

The site is located within close proximity to public bus and train services, as shown in Figure 6. The bus stops identified are serviced by the following bus routes:

- ▶ Stop ID: 002082 & 002083 – Service 360.

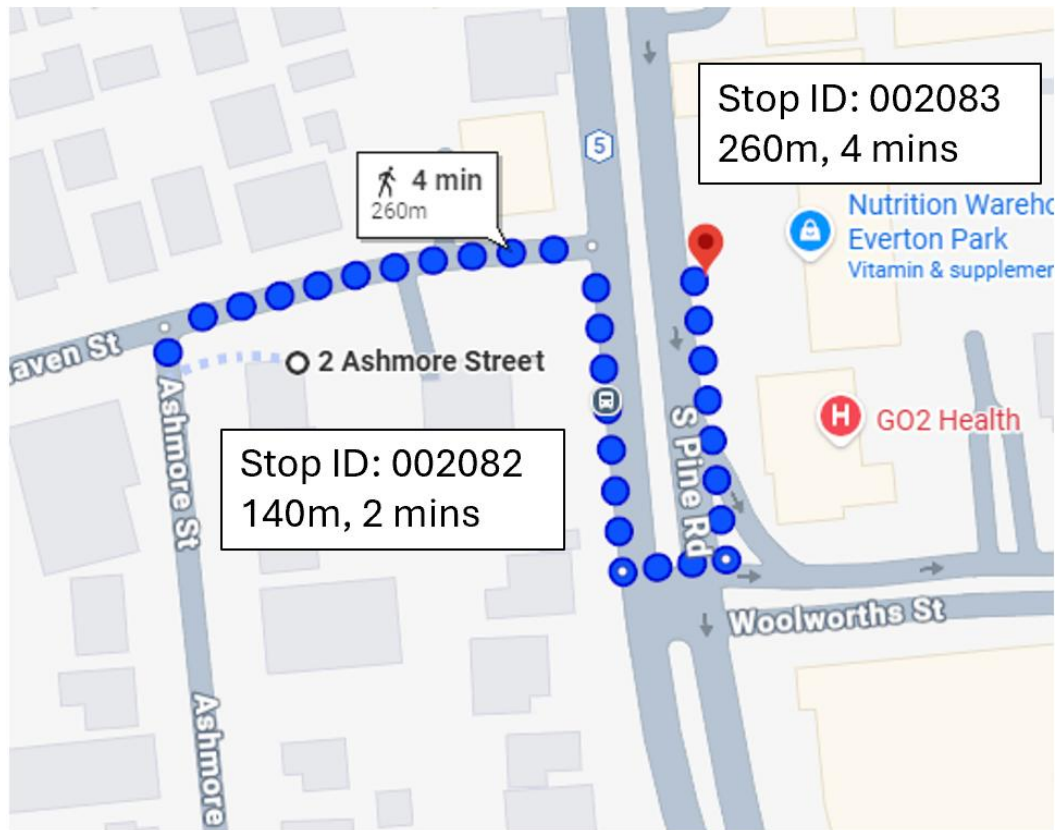


Figure 6: Public Transport

4. Development Traffic Assessment

4.1 Site Access

Site access is proposed from Newhaven Street, as per the existing arrangement.

4.1.1 Site Access Design

As outlined in the BCC City Plan, access to a residential development is to be designed in accordance with Brisbane standard drawing (BSD) 2021 for the following:

- ▶ Driveway type B2 at a minimum of 6.5m wide for access by a refuse collection vehicle (RCV), as shown in Figure 7.

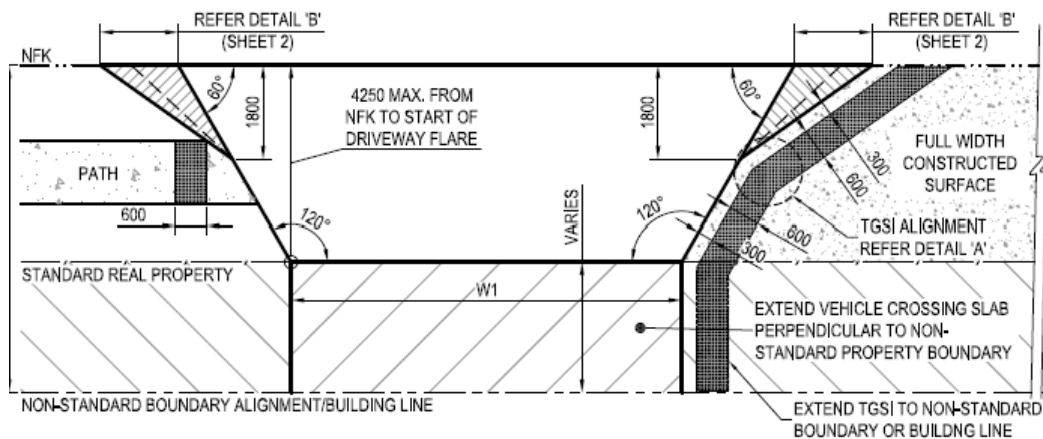
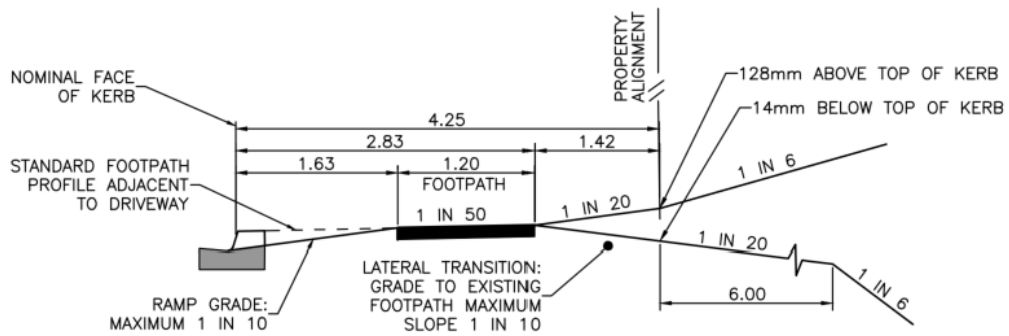


Figure 7: Type B2 Vehicle Crossing

As outlined in the BCC City Plan, the grades for access driveways are to be in accordance with BSD-2021, BSD-2024, BSD-2025, as shown in Figure 8. The standard grades for parking and servicing areas are also as follows:

- ▶ Public and resident parking areas: 1:14 (7.1%).
- ▶ PWD parking areas: 1:40 (2.5%) in all directions.
- ▶ Straight and curved circulation ramps for LRV and RCV: 1:10 (10%).
- ▶ LRV manoeuvring: 1:25 (4%).
- ▶ RCV manoeuvring: 1:20 (5%).



MAXIMUM GRADES
NON-RESIDENTIAL VEHICULAR ENTRANCES &
RESIDENTIAL (OTHER THAN SINGLE DWELLING)

Figure 8: BSD-2025: Vehicle Crossing Grades

The proposed development provides an access driveway and crossover at 6.5m wide, which complies with the above requirements.

4.1.2 Sight Distance

As outlined in the BCC City Plan, the minimum sight distance for the proposed access driveway is:

- ▶ 90m, based on a 50km/h speed environment and an absolute minimum of 50m.

It is noted that Newhaven Street is posted 40km/h area, which is not specified in the City Plan. Due to this the absolute minimum sight distance for a 50km/h speed environment (50m) has been adopted, which also aligns closely with the desirable gap for a 40km/h road speed outlined in AS2890.1.

The available sight distance measured from the proposed site access is approximately 53m to the east and 95m to the west, as shown in Figure 9, which complies with the above requirements.



Figure 9: Access Sight Distance – Newhaven Street

4.2 Parking

4.2.1 Parking Supply

As outlined in the BCC City Plan, the minimum number of off-street parking spaces required for the proposed development is as follows:

- ▶ Short Term Accommodation (48 Units):
 - 0.5 spaces per unit
 - 0.5 spaces per staff
- ▶ Multiple Dwelling (3 units):
 - 1 space per 1 bedroom dwelling
 - 0.25 spaces per dwelling for visitor parking

The proposed development provides 51 off-street parking spaces for residents within a basement plus 12 visitor parking spaces, including 1 PWD space, located outside of the proposed security gate, which exceeds the above requirements.

4.2.2 Parking Design

As outlined in the BCC City Plan, minimum dimensions for reserved (user class 1) and public (user class 2) parking areas with low turnover rates are shown in Table 1.

Table 1: Parking Space Dimensions

User Class	Minimum Length (m)	Minimum Width (m)	Aisle Width (m)
1	5.4	2.4	6.2*
2	5.4	2.5	6.2*
PWD	5.4	2.4 plus 2.4 shared space	6.2*
Small Car	5.0	2.3	6.2*

*0.4m reduction in aisle width for each 0.1m increase in parking bay width

The proposed development provides off-street parking spaces at generally 2.4m wide for both resident and visitors. There are also 8 small car spaces for residents which are marked and comply with the dimensions shown in Table 1.

The use of slightly reduced widths for the visitor parking spaces is deemed suitable to cater for the expected demands of the development due to the following:

- ▶ There is an increased aisle width of 6.5m on entry to the site.
- ▶ Parking space widths align with the requirements for user class 1 and not considered a small car space.

A vehicle swept path analysis has been undertaken which demonstrates the proposed visitor parking area is sufficient to cater for access and manoeuvring for a B85 design vehicle, as shown in Figure 10 and Appendix A.

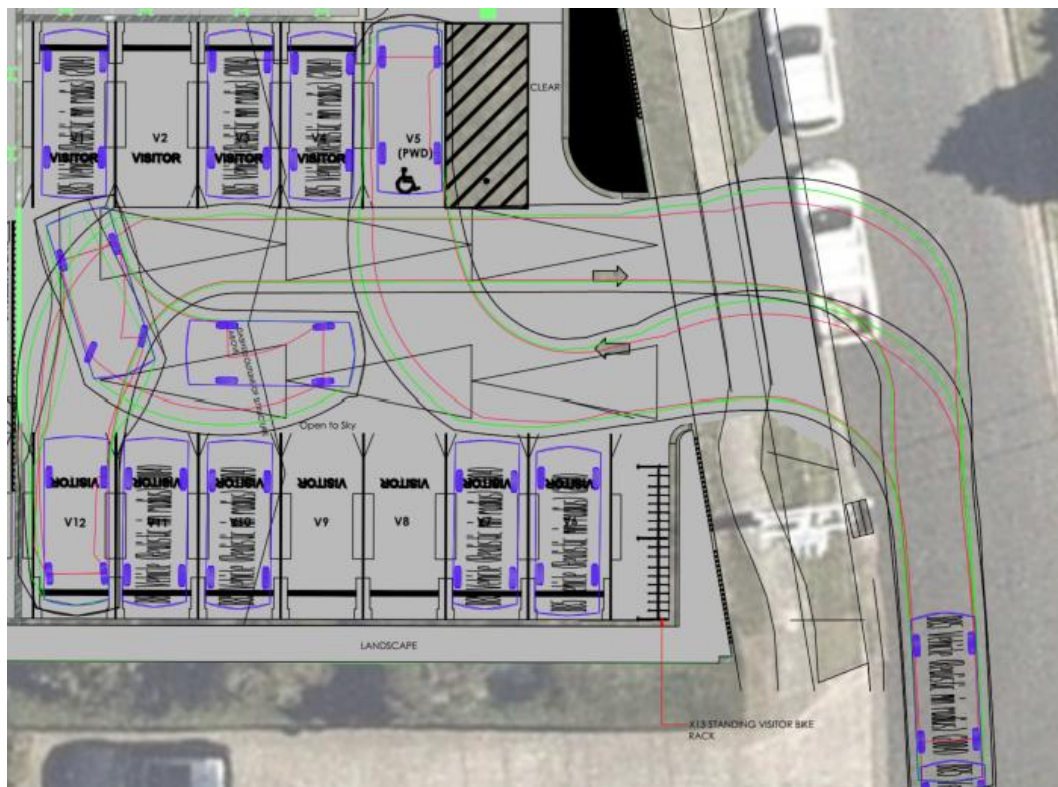


Figure 10: Vehicle Swept Path – B85

4.3 Servicing

As outlined in the BCC City Plan, the minimum class of service vehicle to be accommodated on-site is as follows:

- ▶ Short Term Accommodation:
 - Occasional access for a refuse collection vehicle (RCV).
 - Regular access for a small rigid vehicle (SRV).
- ▶ Multiple Dwelling:
 - Occasional access for a large rigid vehicle (LRV).
 - Regular access for a refuse collection vehicle (RCV).

Due to the scale and nature of the proposed development, on-site access for a LRV is not deemed necessary. Therefore, the largest design service vehicle expected to access the site is a RCV and medium rigid vehicle (MRV).

A vehicle swept path analysis has been undertaken which demonstrates the proposed development is able to cater for safe access and manoeuvring for the largest design service vehicle, as shown in Figure 11 and Appendix A.

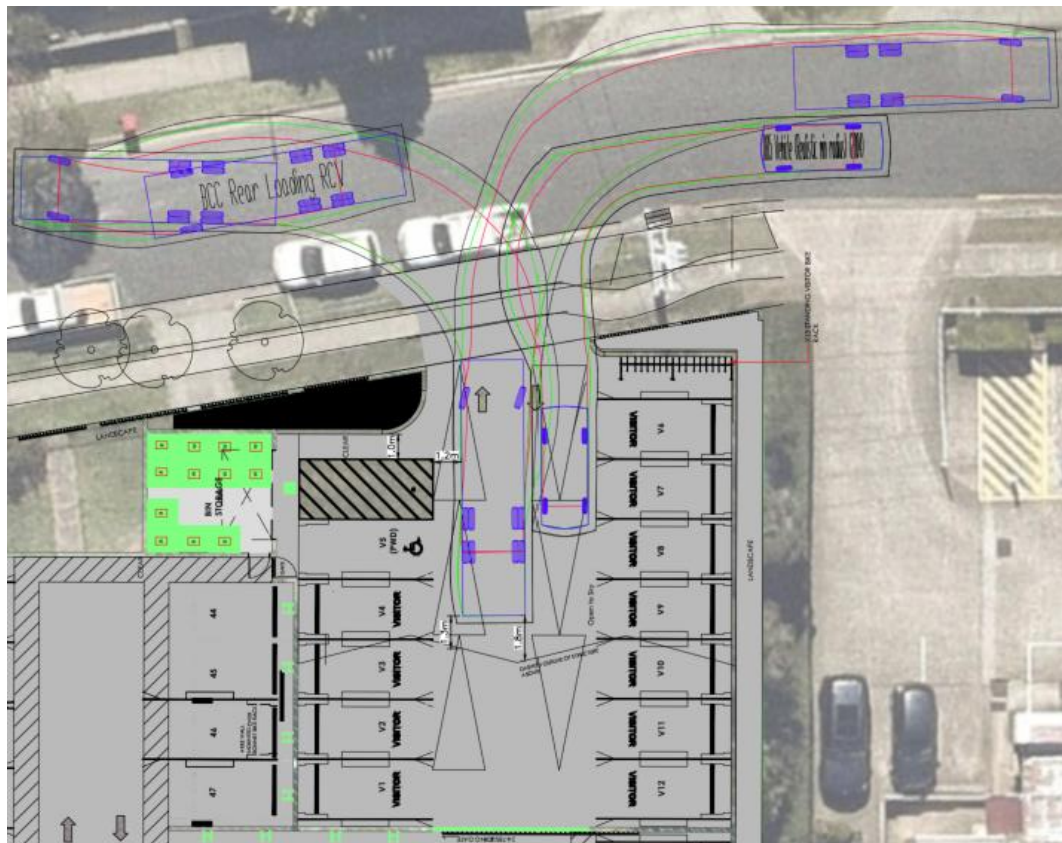


Figure 11: Vehicle Swept Path - RCV

It is noted that the proposed RCV servicing location allows for a minimum 1.2m wide access between the truck and adjacent parking spaces to allow for bin manoeuvring along with appropriate clearance for a passing car.

4.4 Active Transport

As outlined in the BCC City Plan, the standard provision for cyclist and pedestrian facilities is as follows:

- ▶ Short Term Accommodation: Not specified.
- ▶ Multiple Dwelling – 1 lockable covered bicycle parking space per unit, plus 1 visitor bicycle parking space per 4 units.

The proposed development provides 41 bicycle parking spaces within the secured basement car park for residents plus 23 visitor bicycle parking spaces located across both street frontages, which exceeds the above requirements.

5. Traffic Impacts

Details of the proposed development’s expected traffic impacts on the surrounding road network are outlined as follows.

5.1 Estimated Traffic Generation

According to the Guide to Transport Impact Assessment – TfNSW 2024, the traffic generation rates per dwelling for a medium-density residential development are shown in Table 2.

Table 2: Trip Generation Rates (TfNSW Technical Guidance)

Table 5.6. Medium density residential sample summary

Weekday rates	Sydney	Regional
Person trips (person trips/dwelling)		
AM peak hour	1.05	0.93
PM peak hour	0.98	1.21
Daily	6.76	7.04
Vehicle trips (vehicle trips/dwelling)		
AM peak hour	0.39	0.41
PM peak hour	0.37	0.60
Daily	2.72	3.67

The following directional splits are typical of residential subdivisions for the AM and PM peaks movements:

- ▶ AM Peak: 20% In, 80% Out
- ▶ PM Peak: 70% In, 30% Out

Application of the above average trip generation and directional splits results in the following estimated peak hour directional traffic flows of the entire development, as shown in Table 3.

Table 3: Estimated Development Traffic Generation

	No of Dwellings	AM Peak		PM Peak	
		In	Out	In	Out
Residential Dwelling	51	4	16	13	6

5.2 Road Network Impact

The estimated traffic generation outlined above shows that the maximum intensity is expected during the outbound AM peak with approximately 1 vehicle every 4 minutes.

It is expected that most of these movements will turn right out of the site towards South Pine Road, which is a high capacity arterial road. The estimated traffic generation for the proposed development is minimal and, coupled with a left turn only from Newhaven Street onto South Pine Road, is not expected to adversely impact the safe and efficient operation of the surrounding road network.

6. Summary and Recommendation

6.1 Summary

This document presents the findings of a Traffic Impact Assessment for a multiple dwelling development located at 2-6 Ashmore Street, Everton Park. This assessment addresses all relevant traffic and transport items in the Brisbane City Council City Plan, which is summarised as follows:

- ▶ The development provides an access driveway from Newhaven Street at 6.5m wide, designed in compliance with relevant standards.
- ▶ The development provides a total of 51 secured parking spaces for residents and 12 off-street visitor parking spaces, including 1 PWD space, generally designed in compliance with relevant standards.
- ▶ The development is accessed from a minor road and is able to cater for access and manoeuvring by a MRV and RCV.
- ▶ The development provides a total of 41 secured bicycle parking spaces for residents plus 23 bicycle parking spaces for visitors located across both street frontages.
- ▶ The estimated traffic generation for the development is not expected to adversely impact the safe and efficient operation of the local road network.

6.2 Recommendation

Based on our findings of this assessment, and following the implementation of any recommended design amendments, the proposed development will not create any notable traffic impacts to the area and is recommend to be approved based on traffic and transport grounds.

REPORT CERTIFIED BY:



HARRY DEMPSEY

Associate Director | Senior Traffic Engineer

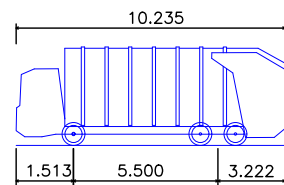
RPEQ # 27009

25 May 2026

Appendix A

VEHICLE SWEPT PATH ANALYSIS

DESIGN VEHICLE

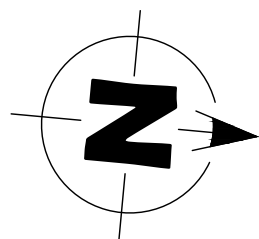


BCC Rear Loading RCV

Overall Length 10.235m
 Overall Width 2.500m
 Overall Body Height 3.600m
 Min Body Ground Clearance 0.304m
 Track Width 2.500m
 Lock-to-lock time 6.00s
 Wall to Wall Turning Radius 9.757m

SWEPT PATH KEY:

- VEHICLE OUTLINE
- VEHICLE OVERHANG
- WHEEL TRACK
- - - 0.3m VEHICLE CLEARANCE

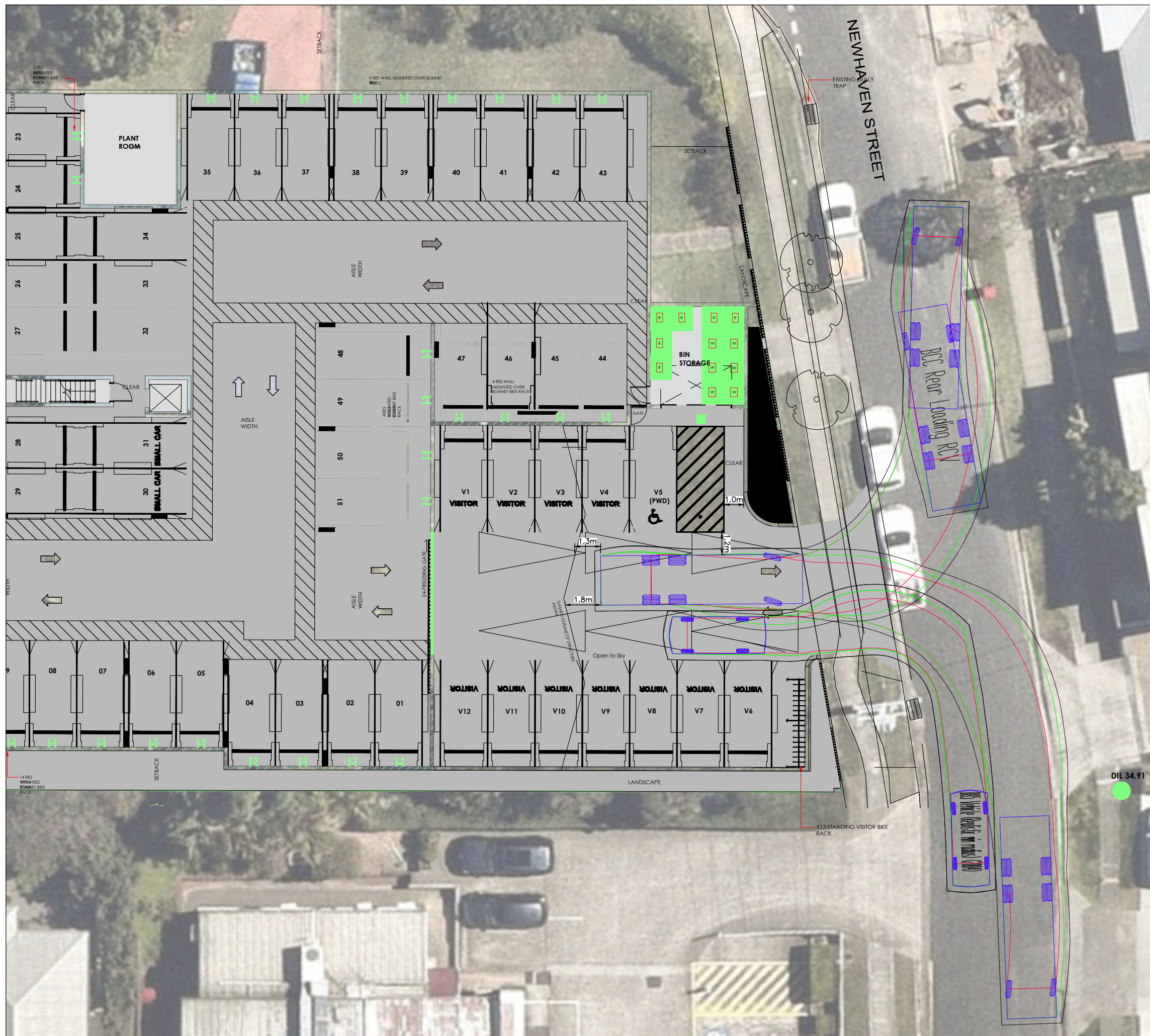


PLAN NUMBER
80669 - SK1

CERTIFIED
H DEMPSEY
 20.05.2026
 RPEQ
 27009

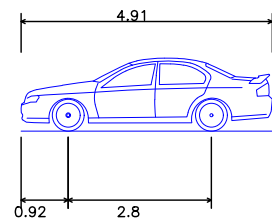
PROJECT NAME
2-6 ASHMORE STREET EVERTON PARK

VEHICLE SWEEP PATH PLAN



PLAN DETAIL
 (SCALE: 1:100)

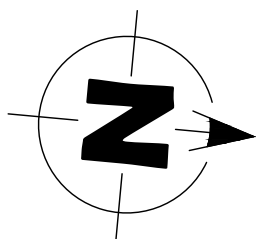
DESIGN VEHICLE



B85 Vehicle
 Overall Length 4.910m
 Overall Width 1.870m
 Overall Body Height 1.421m
 Min Body Ground Clearance 0.159m
 Track Width 1.770m
 Lock-to-lock time 4.00s
 Curb to Curb Turning Radius 5.750m

SWEPT PATH KEY:

- VEHICLE OUTLINE
- VEHICLE OVERHANG
- WHEEL TRACK
- 0.3m VEHICLE CLEARANCE



PLAN NUMBER	CERTIFIED	20.05.2026
80669 - SK2	H DEMPSEY	RPEQ
		27009

PROJECT NAME
2-6 ASHMORE STREET EVERTON PARK

VEHICLE SWEEP PATH PLAN



PLAN DETAIL
 (SCALE: 1:100)