

# Code Compliance Report

804-816 Boundary Road,  
Richlands QLD 4077

# RCS

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**Job No:**  
C26011

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## Document Control Sheet

<b>Project</b>	<b>804 – 816 Boundary Road, Richlands QLD 4077</b>
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# 1. Infrastructure Design Code

## Performance Criteria and Acceptable Solutions

Performance Criteria	Acceptable Solutions	Solution	Comments
<p><b>PO1</b></p> <p>Development provides roads, pavement, edging and landscaping which:</p> <ul style="list-style-type: none"> <li>(a) are designed and constructed in accordance with the road hierarchy;</li> <li>(b) provide for safe travel for pedestrians, cyclists and vehicles;</li> <li>(c) provide access to properties for all modes;</li> <li>(d) provide utilities;</li> <li>(e) provide high levels of aesthetics and amenity, improved liveability and future growth;</li> <li>(f) provide for the amelioration of noise and other pollution;</li> <li>(g) provide a high-quality streetscape;</li> <li>(h) provide a low-maintenance asset with a minimal whole-of-life cost.</li> </ul> <p><i>Note—This can be demonstrated in an engineering report prepared and certified by a Registered Professional Engineer Queensland in accordance with the Infrastructure design planning scheme policy.</i></p>	<p><b>AO1</b></p> <p>Development provides roads and associated pavement, edging and landscaping which are designed and constructed in compliance with the road corridor design standards in the Infrastructure design planning scheme policy.</p>	N/A	<p>Not applicable.</p> <p>No new roads are proposed.</p>
<p><b>PO2</b></p> <p>Development provides road pavement surfaces which:</p> <ul style="list-style-type: none"> <li>(a) are well designed and constructed;</li> <li>(b) durable enough to carry the wheel loads of the intended types and numbers of</li> </ul>	<p><b>AO2</b></p> <p>Development provides road pavement surfaces which are designed and constructed in compliance with the road corridor design standards in the Infrastructure design planning scheme policy.</p>	N/A	<p>Not applicable.</p> <p>Refer PO1 above.</p>

<p>travelling and parked vehicles;</p> <p>(c) ensures the safe passage of vehicles, pedestrians and cyclists, the discharge of stormwater run-off and the <b>preservation of all-weather access</b>;</p> <p>(d) allows for reasonable travel comfort.</p>			
<p><b>PO3</b></p> <p>Development provides a pavement edge which is designed and constructed to:</p> <p>(a) control vehicle movements by delineating the carriageway for all users;</p> <p>(b) provide for people with disabilities by allowing safe passage of wheelchairs and other mobility aids.</p>	<p><b>AO3</b></p> <p>Development provides pavement edges which are designed and constructed in compliance with the road corridor design standards in the Infrastructure design planning scheme policy.</p>	<p>N/A</p>	<p>Not applicable.</p> <p>Refer PO1 above.</p>
<p><b>PO4</b></p> <p>Development provides verges which are designed and constructed to:</p> <p>(a) provide safe access for pedestrians clear of obstructions and access areas for vehicles onto properties;</p> <p>(b) provide a sufficient area for public utility services;</p> <p>(c) be maintainable by the Council.</p>	<p><b>AO4</b></p> <p>Development provides verges which are designed and constructed in compliance with the road corridor design and streetscape locality advice standards in the Infrastructure design planning scheme policy.</p>	<p>A</p>	<p>The proposed development will deliver the standard verge profile along Boundary Road frontage and Kelliher Road frontage of the development in accordance with the infrastructure design planning scheme policy.</p>
<p><b>PO5</b></p> <p>Development provides a lane or laneway identified in a neighbourhood plan which:</p> <p>(a) allows equitable access for all modes;</p> <p>(b) is safe and secure;</p> <p>(c) has 24-hour access;</p> <p>(d) is a low-speed shared zone environment;</p> <p>(e) has a high-quality streetscape.</p>	<p><b>AO5</b></p> <p>Development provides a lane or laneway identified in a neighbourhood plan which is embellished in compliance with the streetscape locality advice standards in the Infrastructure design planning scheme policy.</p>	<p>N/A</p>	<p>Not applicable.</p>

<p><b>PO6</b></p> <p>Development of an existing premises provides at the frontage to the site, if not already provided, the following infrastructure to an appropriate urban standard:</p> <ul style="list-style-type: none"> <li>(a) an effective, high-quality paved roadway;</li> <li>(b) an effective, high-quality roadway kerb and channel;</li> <li>(c) safe, high-quality vehicle crossings over channels and verges;</li> <li>(d) safe, accessible, high-quality verges compatible and integrated with the surrounding environment;</li> <li>(e) safe vehicle access to the site that enables ingress and egress in a forward gear;</li> <li>(f) provision of and required alterations to public utilities;</li> <li>(g) effective drainage;</li> <li>(h) appropriate conduits to facilitate the provision of required street-lighting systems and traffic signals.</li> </ul>	<p><b>AO6</b></p> <p>Development of an existing premises provides at the frontage of the site, if not already existing, the following infrastructure to the standard that would have applied if the development involved new premises as stated in the road corridor design standards in the Infrastructure design planning scheme policy:</p> <ul style="list-style-type: none"> <li>(a) concrete kerb and channel;</li> <li>(b) forming and grading to verges;</li> <li>(c) crossings over channels and verges;</li> <li>(d) a constructed bikeway;</li> <li>(e) a constructed verge or reconstruction of any damaged verge;</li> <li>(f) construction of the carriageway;</li> <li>(g) payment of costs for required alterations to public utility mains, services or installations;</li> <li>(h) construction of and required alterations to public utility mains, services or installations;</li> <li>(i) drainage works;</li> <li>(j) installation of electrical conduits.</li> </ul>	<p>A</p>	<p>The proposed development will deliver the standard verge profile along the Boundary Road frontage and Kelliher Road frontage of the development in accordance with the infrastructure design planning scheme policy</p>
<p><b>PO7</b></p> <p>Development provides both cycle and walking routes which:</p> <ul style="list-style-type: none"> <li>(a) are located, designed and constructed to their network classification (where applicable);</li> <li>(b) provide safe and attractive travel routes for pedestrians and cyclists for commuter</li> </ul>	<p><b>AO7</b></p> <p>Development provides cycle and walking routes which are located, designed and constructed in compliance with the road corridor design and off-road pathway design standards in the Infrastructure design planning scheme policy.</p>	<p>N/A</p>	<p>Not applicable.</p>

<p>and recreational purposes;</p> <p>(c) provide safe and comfortable access to properties for pedestrians and cyclists;</p> <p>(d) incorporate water sensitive urban design into stormwater drainage;</p> <p>(e) provide for utilities;</p> <p>(f) provide for a high level of aesthetics and amenity, improved liveability and future growth;</p> <p>(g) are a low-maintenance asset with a minimal whole-of-life cost;</p> <p>(h) minimise the clearing of significant native vegetation.</p> <p><i>Note—This can be demonstrated in an engineering report prepared and certified by a Registered Professional Engineer Queensland in accordance with the Infrastructure design planning scheme policy.</i></p>			
<p><b>PO8</b></p> <p>Development provides refuse and recycling collection, separation and storage facilities that are located and managed so that adverse impacts on building occupants, neighbouring properties and the public realm are minimised.</p>	<p><b>AO8.1</b></p> <p>Development provides refuse and recycling collection and storage facilities in accordance with the Refuse planning scheme policy.</p>	<p>A/S</p>	<p>Refer to Traffic Impact Assessment Report by Traffic Engineer.</p>
	<p><b>AO8.2</b></p> <p>Development ensures that refuse and recycling collection and storage location and design do not have any adverse impact including odour, noise or visual impacts on the amenity of land uses within or adjoining the development.</p> <p><i>Note—Refer to the Refuse planning scheme policy for further guidance.</i></p>	<p>A</p>	<p>Refer AO8.1 above.</p>

<p><b>PO9</b></p> <p>Development ensures that:</p> <p>(a) land used for an urban purpose is serviced adequately with regard to water supply and waste disposal;</p> <p>(b) the water supply meets the stated standard of service for the intended use and fire-fighting purposes.</p>	<p><b>AO9.1</b></p> <p>Development ensures that the reticulated water and sewerage distribution system for all services is in place before the first use is commenced.</p>	A	<p>Water services are proposed for the development with proposed connections to the existing watermain within Kelliher Road</p> <p>Sewer property connection is proposed for the development with consent for connections to the existing main within Kelliher Road</p>
	<p><b>AO9.2</b></p> <p>Development provides the lot with reticulated water supply and sewerage to a standard acceptable to the distributor-retailer.</p>	A	Refer AO9.1 above.
<p><b>PO10</b></p> <p>Development provides public utilities and street lighting which are the best current or alternative technology and facilitate accessibility, easy maintenance, minimal whole-of-life costs, and minimal adverse environmental impacts.</p>	<p><b>AO10.1</b></p> <p>Development provides public utilities and street lighting which are located and aligned to:</p> <p>(a) avoid significant native vegetation and areas identified within the Biodiversity areas overlay map;</p> <p>(b) minimise earthworks;</p> <p>(c) avoid crossing waterways, waterway corridors and wetlands or if a crossing is unavoidable, tunnel-boring techniques are used to minimise disturbance, and a disturbed area is reinstated and restored on completion of the work.</p>	N/A	Not applicable.
	<p><b>AO10.2</b></p> <p>Development provides compatible public utility services and street-lighting services which are co-located in common trenching for underground services.</p>	N/A	Not applicable.

	<p><b>AO10.3</b> Development provides public utilities and street lighting which are designed and constructed in compliance with the public utilities standards in the Infrastructure design planning scheme policy.</p>	N/A	Not applicable.
<p><b>PO11</b> Development ensures that land used for urban purposes is serviced adequately with telecommunications and energy supply.</p>	<p><b>AO11</b> Development provides land with the following services to the standards of the approved supplier:</p> <ul style="list-style-type: none"> <li>(a) electricity;</li> <li>(b) telecommunications services;</li> <li>(c) gas services where practicable</li> </ul>	A	<p>Existing telecommunication and electrical supply infrastructure exists within the Boundary Road &amp; Kelliher Road Reserve.</p> <p>The details of proposed connections will be documented by the electrical consultant during the detailed design phase.</p>
<p><b>PO12</b> Development ensures that major public projects promote the provision of affordable, high-bandwidth telecommunications services throughout the city.</p>	<p><b>AO12</b> Development provides conduits which are provided in all major Council and government works projects to enable the future provision of fibre optic cabling, if:</p> <ul style="list-style-type: none"> <li>(a) the additional expense is unlikely to be prohibitive; or</li> <li>(b) further major work is unlikely or disruption would be a major concern, such as where there is a limited capacity road; or</li> <li>(c) there is a clear gap in the telecommunications network; or</li> <li>(d) there is a clear gap in the bandwidth available to the area.</li> </ul>	A/S	<p>Existing telecommunication and electrical supply infrastructure exists within the Boundary Road &amp; Kelliher Road Reserve.</p> <p>The details of proposed connections will be documented by the electrical consultant during the detailed design phase.</p>

<p><b>PO13</b></p> <p>Development provides public art identified in a neighbourhood plan or park concept plan which:</p> <ul style="list-style-type: none"> <li>(a) is provided commensurate with the status and scale of the proposed development;</li> <li>(b) is sited and designed: <ul style="list-style-type: none"> <li>(i) as an integrated part of the project design;</li> <li>(ii) as conceptually relevant to the context of the location;</li> <li>(iii) to reflect and respond to the cultural values of the community;</li> <li>(iv) to promote local character in a planned and informed manner.</li> </ul> </li> </ul>	<p><b>AO13</b></p> <p>Development provides public art identified in a neighbourhood plan or park concept plan which is sited and designed in compliance with the public art standards in the Infrastructure design planning scheme policy.</p>	<p>N/A</p>	<p>Not applicable.</p>
<p><b>PO14</b></p> <p>Development provides signage of buildings and spaces which promote legibility to help users find their way.</p>	<p><b>AO14</b></p> <p>Development provides public signage:</p> <ul style="list-style-type: none"> <li>(a) at public transport interchanges and stops, key destinations, public spaces, pedestrian linkages and at entries to centre developments;</li> <li>(b) which details the location of the key destinations, public spaces and pedestrian linkages in the vicinity, the services available within the development and where they are located.</li> </ul>	<p>N/A</p>	<p>Not applicable.</p>
<p><b>PO15</b></p> <p>Development that provides community facilities which form part of the development is functional, safe, low maintenance, and fit for purpose.</p>	<p><b>AO15</b></p> <p>Development that provides community facilities which form part of the development is designed in compliance with the community facilities standards in the Infrastructure design planning scheme policy.</p>	<p>N/A</p>	<p>Not applicable.</p>

<p><b>PO16</b></p> <p>Development provides public toilets which:</p> <ul style="list-style-type: none"> <li>(a) are required as part of a community facility or park;</li> <li>(b) are located, designed and constructed to be: <ul style="list-style-type: none"> <li>(i) safe;</li> <li>(ii) durable;</li> <li>(iii) resistant to vandalism;</li> <li>(iv) able to service expected demand;</li> <li>(v) fit for purpose.</li> </ul> </li> </ul>	<p><b>AO16</b></p> <p>Development that provides public toilets is designed and constructed in compliance with the public toilets standards in the Infrastructure design planning scheme policy.</p>	<p>N/A</p>	<p>Not applicable.</p>
<p><b>PO17</b></p> <p>Development provides bridges, tunnels, elevated structures and water access structures that are designed and constructed using proven methods, materials and technology to provide for:</p> <ul style="list-style-type: none"> <li>(a) safe movement of intended users;</li> <li>(b) an attractive appearance appropriate to the general surroundings and any adjacent structures;</li> <li>(c) functionality and easy maintenance;</li> <li>(d) minimal whole-of-life cost;</li> <li>(e) longevity;</li> <li>(f) current and future services.</li> </ul> <p><i>Note—All bridges and elevated and associated elements must be designed and certified by a Registered Professional Engineer Queensland in accordance with the Infrastructure design planning scheme policy.</i></p>	<p><b>AO17</b></p> <p>Development that provides bridges, tunnels, elevated structures and water access structures is designed and constructed in compliance with the standards in the Infrastructure design planning scheme policy.</p>	<p>N/A</p>	<p>Not applicable.</p>

<p><b>PO18</b></p> <p>Development provides culverts which are designed and constructed using proven methods, materials and technology to provide for:</p> <ul style="list-style-type: none"> <li>(a) safety;</li> <li>(b) an attractive appearance appropriate to the general surroundings;</li> <li>(c) functionality and easy maintenance;</li> <li>(d) minimal whole-of-life cost;</li> <li>(e) longevity;</li> <li>(f) future widening;</li> <li>(g) current and future services;</li> <li>(h) minimal adverse impacts, such as increase in water levels or flow velocities, and significant change of flood patterns.</li> </ul> <p><i>Note—All culverts and associated elements are to be designed and certified by a Registered Professional Engineer Queensland in accordance with the applicable design standards.</i></p>	<p><b>AO18</b></p> <p>Development that provides culverts is designed and constructed in compliance with the structures standards in the Infrastructure design planning scheme policy.</p>	<p>N/A</p>	<p>Not applicable.</p>
<p><b>PO19</b></p> <p>Development provides batters, retaining walls, and seawalls and river walls which are designed and constructed using proven methods, materials and technology to provide for:</p> <ul style="list-style-type: none"> <li>(a) safety;</li> <li>(b) an attractive appearance appropriate to the surrounding area;</li> <li>(c) easy maintenance;</li> <li>(d) minimal whole-of-life cost;</li> <li>(e) longevity;</li> <li>(f) minimal water seepage.</li> </ul>	<p><b>AO19</b></p> <p>Development that provides batters, retaining walls, seawalls and river walls is designed and constructed in compliance with the structures standards in the Infrastructure design planning scheme policy.</p>	<p>A</p>	<p>Retaining walls to be designed by the structural engineer in accordance with the BCC Infrastructure design planning scheme policy.</p>

If for development with a gross floor area greater than 1,000m <sup>2</sup>			
<p><b>PO20</b></p> <p>Development ensures that construction is managed so that use of public spaces and movement on pedestrian, cyclist and other traffic routes is not unreasonably disrupted and existing landscaping is adequately protected from short- and long-term impacts.</p> <p><i>Note—The preparation of a construction management plan can assist in demonstrating achievement of this performance outcome.</i></p> <p><i>Note—The Transport, access, parking and servicing planning scheme policy provides advice on the management of vehicle parking and deliveries during construction.</i></p>	<p><b>AO20</b></p> <p>Development ensures that during construction:</p> <ul style="list-style-type: none"> <li>(a) the ongoing use of adjoining and surrounding parks and public spaces, such as malls and outdoor dining, is not compromised;</li> <li>(b) adjoining and surrounding landscaping is protected from damage;</li> <li>(c) safe, legible, efficient and sufficient pedestrian, cyclist and vehicular accessibility and connectivity to the wider network are maintained.</li> </ul>	<p>A/S</p>	<p>Contractor to implement a construction management plan to effectively minimise risks and delays to the community during construction.</p>
<p><b>PO21</b></p> <p>Development ensures that construction and demolition activities are guided by measures that prevent or minimise adverse impacts including sleep disturbance at a sensitive use, due to noise and dust, including dust from construction vehicles entering and leaving the site.</p> <p><i>Note—A noise and dust impact management plan prepared in accordance with the Management plans planning scheme policy can assist in demonstrating achievement of this performance outcome.</i></p>	<p><b>AO21.1</b></p> <p>Development ensures that demolition and construction:</p> <ul style="list-style-type: none"> <li>(a) only occur between 6:30am and 6:30pm Monday to Saturday, excluding public holidays;</li> <li>(b) do not occur over periods greater than 6 months.</li> </ul>	<p>A/S</p>	<p>Contractor to implement a construction management plan to effectively minimise risks and delays to the community during construction.</p>
	<p><b>AO21.2</b></p> <p>Development including construction and demolition does not release dust emissions beyond the boundary of the site.</p>	<p>A/S</p>	<p>Contractor to implement a construction management plan to effectively minimise the release of dust beyond the boundary during construction.</p>
	<p><b>AO21.3</b></p> <p>Development construction and demolition does not involve asbestos-containing materials.</p>	<p>A/S</p>	<p>Contractor to employ licensed businesses to undertake high risk asbestos removal activities</p>

<p><b>PO22</b></p> <p>Development ensures that:</p> <ul style="list-style-type: none"> <li>(a) construction and demolition do not result in damage to surrounding property as a result of vibration;</li> <li>(b) vibration levels achieve the vibration criteria in Table 9.4.4.3.B, Table 9.4.4.3.C, Table 9.4.4.3.D and Table 9.4.4.3.E.</li> </ul> <p><i>Note—A vibration impact assessment report prepared in accordance with the Noise impact assessment planning scheme policy can assist in demonstrating achievement of this performance outcome.</i></p>	<p><b>AO22</b></p> <p>Development ensures that the nature and scale of construction and demolition do not generate noticeable levels of vibration.</p>	<p>A/S</p>	<p>Contractor to implement a construction management plan to effectively minimise the generation of vibration during construction.</p>
<p><b>If for a material change of use or reconfiguring a lot in an urban area (as defined in the Regulation) involving premises that is, or will be, accessed by common private title, where involving buildings, either attached or detached, that are not covered by other legislation mandating fire hydrants</b></p>			
<p><b>PO23</b></p> <p>Development ensures that fire hydrants are:</p> <ul style="list-style-type: none"> <li>(a) installed and located to enable fire services to access water safely, effectively and efficiently;</li> <li>(b) suitably identified so that fire services can locate them at all hours.</li> </ul>	<p><b>AO23.1</b></p> <p>Above or below ground fire hydrants are provided on residential, commercial and industrial streets and private roads, at not more than 90m intervals, and at each street intersection.</p> <p><i>Note—On residential streets, above ground fire hydrants may be single outlet. On commercial and industrial streets above ground fire hydrants should have dual valved outlets.</i></p>	<p>A/S</p>	<p>The proposed development will provide a private fire system designed by the hydraulic consultant during detailed design.</p>
	<p><b>AO23.2</b></p> <p>Fire hydrants are identified by:</p> <ul style="list-style-type: none"> <li>(a) raised reflectorised pavement markers (RRPM) on sealed roads;</li> <li>(b) marker posts at the fence line where on an unsealed road, as road (HR) or path (HP) hydrants</li> </ul>	<p>N/A</p>	<p>Refer to AO23.1 above</p>

<p><b>PO24</b></p> <p>Development ensures road widths and construction within the development, are adequate for refuse vehicles and for fire emergency vehicles to gain access to a safe working area close to buildings and near water supplies whether or not on-street parking spaces are occupied.</p>	<p><b>AO24</b></p> <p>Internal private roads have a minimum roadway clearance between obstructions of 3.5m wide and 4.8m high in addition to any width required for on-street parking.</p>	<p>N/A</p>	<p>Not applicable.</p>
<p><b>Development for major electricity infrastructure and bulk water supply infrastructure identified on the State Planning Policy Interactive Mapping System where not in the Utility services zone precinct of the Special purpose zone</b></p>			
<p><b>PO25</b></p> <p>Development avoids or otherwise minimises adverse impacts on surrounding land uses through the use of buffers and setbacks and the appropriate design and location of plant and operational areas within the site.</p>	<p><b>AO25</b></p> <p>No acceptable outcome is prescribed.</p>	<p>N/A</p>	<p>Not applicable.</p>
<p><b>Development potentially impacting on major electricity infrastructure and bulk water supply infrastructure identified on the State Planning Policy Interactive Mapping System where the infrastructure is not in the Utility services zone precinct of the Special purpose zone</b></p>			
<p><b>PO26</b></p> <p>Development is sited and designed to:</p> <ul style="list-style-type: none"> <li>(a) avoid safety risks to people or property;</li> <li>(b) minimise noise and visual impacts to people and property;</li> <li>(c) ensure the physical integrity and operation, maintenance and expansion of the infrastructure is not compromised.</li> </ul>	<p><b>AO26</b></p> <p>No acceptable outcome is prescribed.</p>	<p>N/A</p>	<p>Not applicable.</p>

**Solution Legend**

- A = Acceptable solution
- A/S = Alternate solution proposed
- N/A = Not applicable to this application

## 2. Filling and Excavation Code

### Performance Criteria and Acceptable Solutions

Performance Criteria	Acceptable Solutions	Solution	Comments
<p><b>PO1</b></p> <p>Development for filling or excavation minimises visual impacts from retaining walls and earthworks.</p>	<p><b>AO1</b></p> <p>Development ensures that the total height of any cut and fill, whether or not retained, does not exceed:</p> <ul style="list-style-type: none"> <li>(a) 2.5m in a zone in the Industry zones category;</li> <li>(b) 1m in all other zones, or if adjoining a sensitive zone.</li> </ul>	A/S	<p>The required filling and excavation works will be limited to the proposed new RP boundary. There are a some of fill walls in excess of the 1.0m height however the amenity impacts of these structures will be isolated to the development site.</p> <p>All structures will be designed by a suitably qualified engineer.</p>
<p><b>PO2</b></p> <p>Development of a retaining wall proposed as a result of filling or excavation:</p> <ul style="list-style-type: none"> <li>(a) is designed and constructed to be fit for purpose;</li> <li>(b) does not impact adversely on significant vegetation;</li> <li>(c) is capable of easy maintenance. gradients will need to comply with the Building Regulation.</li> </ul> <p><i>Note—Guidance on the protection of native vegetation is included in the Biodiversity areas planning scheme policy.</i></p>	<p><b>AO2.1</b></p> <p>Development of a retaining structure, including footings, surface drainage and subsoil drainage:</p> <ul style="list-style-type: none"> <li>(a) is wholly contained within the site;</li> <li>(b) if the total height to be retained is greater than 1m, then: <ul style="list-style-type: none"> <li>(i) the retaining wall at the property boundary is no greater than 1m above the ground level;</li> <li>(ii) all further terracing from the 1m high boundary retaining wall is 1 vertical unit:1 horizontal unit;</li> <li>(iii) the distance between each successive retaining wall (back of lower wall to face of higher wall) is no less than 1m horizontally to incorporate planting areas.</li> </ul> </li> </ul>	A/S	<p>Subsoil drainage will be captured and directed to the internal stormwater system.</p>

	<p><b>AO2.2</b> Development of a retaining wall over 1m in height protects significant vegetation on the site and on adjoining land and is designed and constructed in accordance with the structures standards in the Infrastructure design planning scheme policy and certified by a Registered Professional Engineer Queensland.</p>	N/A	<p>Not applicable. No significant vegetation has been identified on the subject site.</p>
	<p><b>AO2.3</b> Development provides a retaining wall finish that presents to adjoining land that is maintenance free if the setback is less than 750mm from the boundary.</p>	A	<p>Proposed retaining structures walls to be constructed from materials ensuring a 60 year design life.</p>
	<p><b>AO2.4</b> Development for filling only uses clean fill that does not include any construction rubble or debris.</p>	A	<p>Any imported fill expected for the site will be clean fill that does not contain any debris or construction rubble.</p>
<p><b>PO3</b> Development ensures that a rock anchor is designed and constructed to be fit for purpose.</p>	<p><b>AO3</b> Development ensures that a rock anchor: (a) is constructed in accordance with the standards in the Infrastructure design planning scheme policy; (b) where it extends beyond the property boundary, is supported by a letter of consent from the adjoining land and building owners.</p>	N/A	<p>Not applicable. No rock anchors are proposed.</p>
<p><b>PO4</b> Development protects all services and public utilities.</p>	<p><b>AO4</b> Development protects services and public utilities and ensures that any alteration or relocation of services or public utilities meets the standard design specifications of the responsible service authorities.</p>	A	<p>Prior to commencement of construction the contractor will be required to satisfy themselves of the correct locations of all existing services whether indicated or not on the plans. Construction of the proposed development will ensure the protection of all existing services and public utilities.</p>

<p><b>PO5</b></p> <p>Development provides surface and sub-surface drainage to prevent water seepage, concentration of run-off or ponding of stormwater on adjacent land.</p>	<p><b>AO5</b></p> <p>Development ensures all flows and subsoil drainage are directed to a lawful point of discharge of a surface water diversion drain, including to the top or toe of a retaining wall in accordance with the stormwater drainage section of the Infrastructure design planning scheme policy.</p>	<p>A</p>	<p>It is proposed to discharge roof water from the development directly into the stormwater infrastructure on Kelliher Road as the lawful point of discharge.</p> <p>It is proposed to discharge seepage flows captured by basement retention into the internal basement pump system and to the lawful point of discharge.</p>
<p><b>PO6</b></p> <p>Development ensures that the design and construction of all open drainage works is undertaken in accordance with natural channel design principles, being the development of a stormwater conveyance system for major flows, by using a vegetated open channel or drain that approximates the features and functions of a natural waterway to enhance or improve riparian values of those stormwater conveyance systems. <i>Editor's note—Guidance on natural channel design principles can be found in the Council's publication Natural channel design guidelines.</i></p>	<p><b>AO6</b></p> <p>No acceptable outcome is prescribed.</p>	<p>N/A</p>	<p>Not applicable.</p>
<p><b>PO7</b></p> <p>Development for filling or excavation:</p> <ul style="list-style-type: none"> <li>(a) does not degrade water quality or adversely affect environmental values in receiving waters;</li> <li>(b) ensures site sediment and erosion control standards are best practice.</li> </ul>	<p><b>AO7.1</b></p> <p>Development for filling or excavation provides water quality treatment that complies with the stormwater drainage section of the Infrastructure design planning scheme policy.</p>	<p>A</p>	<p>An Erosion and Sediment Control Plan shall be prepared (prior to commencement of construction) and maintained for the management of sediment laden stormwater during the construction phase in accordance with the stormwater drainage section of the Infrastructure Design Planning Scheme Policy.</p>

	<p><b>AO7.2</b> Development provides erosion and sediment control standards that are in accordance with the stormwater drainage section of the Infrastructure design planning scheme policy.</p>	A	Refer AO7.1 above.
<p><b>PO8</b> Development for filling or excavation is conducted such that adverse impacts at a sensitive use due to noise and dust are prevented or minimised. <i>Note—A noise and dust impact management plan prepared in accordance with the Management plans planning scheme policy can assist in demonstrating achievement of this performance outcome.</i></p>	<p><b>AO8.1</b> Development ensures that no dust emissions extend beyond the boundary of the site, including dust from construction vehicles entering and leaving the site.</p>	A	Contractor to implement dust suppression measures for the duration of works to ensure dust emissions are controlled.
	<p><b>AO8.2</b> Development for filling or excavation activity only occurs between the hours of 6:30am and 6:30pm Monday to Saturday, excluding public holidays.</p>	A	Contractor to undertake works within the timeframe of 6:30am to 6:30pm Monday to Saturday.
<p><b>PO9</b> Development ensures that vibration generated by the filling or excavation operation does not exceed the vibration criteria in Table 9.4.3.3.D, Table 9.4.3.3.E, Table 9.4.3.3.F and Table 9.4.3.3.G. <i>Note—A noise management report prepared in accordance with the Noise impact assessment planning scheme policy can assist in demonstrating achievement of this performance outcome.</i></p>	<p><b>AO9</b> Development involving filling or excavation does not cause a ground-borne vibration beyond the boundary of the site.</p>	A	Contractor to ensure only static compaction measures are used in the placement of fill.
<p><b>PO10</b> Development ensures that heavy trucks hauling material to and from the site do not affect the amenity of established areas and limits environmental nuisance impact on adjacent land.</p>	<p><b>AO10</b> Development ensures that heavy trucks hauling material to and from the site:</p> <ul style="list-style-type: none"> <li>(a) occur for a maximum of 3 weeks;</li> <li>(b) use a major road to access the site;</li> <li>(c) only use a minor road for the shortest-most-direct route that has the least amount of environmental nuisance if there is no major road alternative.</li> </ul>	A	Contractor to ensure works are undertaken in an efficient manner to limit the period of material haulage. Access to the site is to be managed by the contractor to ensure pedestrian and motorist safety.

<p><b>PO11</b></p> <p>Development for filling or excavation protects the environment and community health and wellbeing from exposure to contaminated land and contaminated material.</p>	<p><b>AO11</b></p> <p>Development does not involve:</p> <p>(a) excavation on land previously occupied by a notifiable activity or on land listed on the Environmental Management Register or the Contaminated Land Register;</p> <p>(b) filling with material containing a contaminant.</p>	<p>N/A</p>	<p>A search of BCC's interactive mapping indicates that the site is not inside the areas where potential and actual acid sulphate soils may be encountered. Contractor to engage suitably qualified Geotechnical engineer if acid sulphate soils are encountered. A search by the contractor will determine if the land is on the Contaminated Land Register.</p>
<p><b>PO12</b></p> <p>Development provides for:</p> <p>(a) landscaping for water conservation purposes;</p> <p>(b) water sensitive urban design measures which are employed within the landscape design to maximise stormwater use and to reduce any adverse impacts on the landscape;</p> <p>stormwater harvesting to be maximised and any adverse impacts of stormwater minimised.</p>	<p><b>AO12.1</b></p> <p>Development provides landscaping which is designed using the standards in the Landscape design guidelines for water conservation planning scheme policy.</p>	<p>A</p>	<p>Refer to Landscape Plans provided by others.</p>
	<p><b>AO12.2</b></p> <p>Development ensures that the design and requirements for irrigation are in compliance with the standards in the Landscape design guidelines for water conservation planning scheme policy.</p>	<p>A</p>	<p>Refer to Landscape Plans provided by others.</p>
	<p><b>AO12.3</b></p> <p>Development provides areas of pavement, turf and mulched garden beds which are drained. Note— This may be achieved through the provision and/or treatment of swales, spoon drains, field gullies, sub-surface drainage and stormwater connections.</p>	<p>A</p>	<p>Refer to Landscape Plans provided by others.</p>
<p><b>PO13</b></p> <p>Development ensures cutting and filling for the development of canals or artificial waterways avoids adverse impacts on coastal resources and processes.</p>	<p><b>AO13</b></p> <p>Development does not involve the creation of canals or artificial waterways.</p>	<p>N/A</p>	<p>Not Applicable.</p>

- A = Acceptable solution  
A/S = Alternate solution proposed  
N/A = Not applicable to this application

### 3. Stormwater Code

#### Performance Criteria and Acceptable Solutions

Performance Criteria	Acceptable Solutions	Solution	Comments
<p><b>Section A—If for a material change of use, reconfiguring a lot, operational work or building work</b></p> <p><i>Note—Compliance with the performance outcomes and acceptable outcomes in this section should be demonstrated by the submission of a site-based stormwater management plan for high risk development only.</i></p>			
<p><b>PO1</b></p> <p>Development provides a stormwater management system which achieves the integrated management of stormwater to:</p> <ul style="list-style-type: none"> <li>(a) minimise flooding;</li> <li>(b) protect environmental values of receiving waters;</li> <li>(c) maximise the use of water sensitive urban design;</li> <li>(d) minimise safety risk to all persons;</li> <li>(e) maximise the use of natural waterway corridors and natural channel design principles.</li> </ul> <p><i>Editor's note—The stormwater management system to be developed to address PO1 is not intended to require management of stormwater quality.</i></p>	<p><b>AO1</b></p> <p>Development provides a stormwater management system designed in compliance with the Infrastructure design planning scheme policy.</p>	A	In accordance with the BCC Infrastructure design planning scheme policy roof water flows will be directed to the existing stormwater infrastructure on Kelliher Road as a lawful point of discharge.
<p><b>PO2</b></p> <p>Development ensures that the stormwater management system and site work does not adversely impact flooding or drainage characteristics of premises which are up slope, down slope or adjacent to the site.</p>	<p><b>AO2.1</b></p> <p>Development does not result in an increase in flood level or flood hazard on up slope, down slope or adjacent premises.</p>	A	The proposed development will discharge roof water to the existing stormwater infrastructure on Kelliher Road as the lawful point of discharge in accordance with the BCC infrastructure design planning scheme policy.

	<p><b>AO2.2</b></p> <p>Development provides a stormwater management system which is designed in compliance with the standards in the Infrastructure design planning scheme policy.</p>	A	Refer AO1 above.
<p><b>PO3</b></p> <p>Development ensures that the stormwater management system does not direct stormwater run-off through existing or proposed lots and property where it is likely to adversely affect the safety of, or cause nuisance to properties.</p>	<p><b>AO3.1</b></p> <p>Development ensures that the location of the stormwater drainage system is contained within a road reserve, drainage reserve, public pathway, park or waterway corridor.</p>	A	The stormwater drainage system is a private asset and the outlet to the lawful point of discharge is the kerb and channel on Kelliher Road.
	<p><b>AO3.2</b></p> <p>Development provides a stormwater management system which is designed in compliance with the standards in the Infrastructure design planning scheme policy.</p>	A	Refer AO3.1 above.
	<p><b>AO3.3</b></p> <p>Development obtains a lawful point of discharge in compliance with the standards in the Infrastructure design planning scheme policy.</p>	A	Refer AO3.1 above.
	<p><b>AO3.4</b></p> <p>Where on private land, all underground stormwater infrastructure is secured by a drainage easement.</p>	N/A	Not applicable.
<p><b>PO4</b></p> <p>Development provides a stormwater management system which has sufficient capacity to safely convey run-off taking into account increased run-off from impervious surfaces and flooding in local catchments.</p>	<p><b>AO4.1</b></p> <p>Development provides a stormwater conveyance system which is designed to safely convey flows in compliance with the standards in the Infrastructure design planning scheme policy.</p>	A	Refer AO1 above.

	<p><b>AO4.2</b> Development provides sufficient area to convey run-off which will comply with the standards in the Infrastructure design planning scheme policy.</p>	A	Refer AO1 above.
<p><b>PO5</b> Development designs stormwater channels, creek modification works, bridges, culverts and major drains to protect and enhance the value of the waterway corridor or drainage path for fauna movement.</p>	<p><b>AO5</b> Development ensures the design of stormwater channels, creek modifications or other infrastructure, permits terrestrial and aquatic fauna movement.</p>	N/A	Not applicable.
<p><b>PO6</b> Development ensures that location and design of stormwater detention and water quality treatment:</p> <ul style="list-style-type: none"> <li>(a) minimises risk to people and property;</li> <li>(b) provides for safe access and maintenance;</li> <li>(c) minimises ecological impacts to creeks and waterways.</li> </ul>	<p><b>AO6.1</b> Development locates stormwater:</p> <ul style="list-style-type: none"> <li>(a) detention and water quality treatment:</li> <li>(b) outside of a waterway corridor;</li> <li>(c) offline to any catchment not contained within the development.</li> </ul>	N/A	Not applicable.
	<p><b>AO6.2</b> Development providing for stormwater detention and water quality treatment devices are designed in compliance with the standards in the Infrastructure design planning scheme policy.</p>	N/A	Not applicable.
<p><b>PO7</b> Development is designed, including any car parking areas and channel works to:</p> <ul style="list-style-type: none"> <li>(a) reduce property damage;</li> <li>(b) provide safe access to the site during the defined flood event.</li> </ul>	<p><b>AO7.1</b> Development (including any ancillary structures and car parking areas) is located above minimum flood immunity levels in Table 9.4.9.3.B, Table 9.4.9.3.C, Table 9.4.9.3.D, Table 9.4.9.3.E and Table 9.4.9.3.F.</p> <p><i>Note—Compliance with this acceptable outcome can be demonstrated by the submission of a hydraulic and hydrology report identifying flood levels and development design levels (as part of a site-based stormwater management plan).</i></p>	A	<p>Searches of the BCC Flood Awareness Mapping shows that the site is outside of areas of expected flooding.</p> <p>Furthermore, the BCC Floodwise Property Report for the site has no flags for building or development purposes.</p>

	<p><b>AO7.2</b> Development including the road network provides a stormwater management system that provides safe pedestrian and vehicle access in accordance with the standards in the Infrastructure design planning scheme policy.</p>	A	Refer AO7.1 above.
<p><b>PO8</b> Development designs stormwater channels, creek modification works and the drainage network to protect and enhance the environmental values of the waterway corridor or drainage path.</p>	<p><b>AO8.1</b> Development ensures natural waterway corridors and drainage paths are retained.</p>	N/A	Not applicable.
	<p><b>AO8.2</b> Development provides the required hydraulic conveyance of the drainage channel and floodway, while maximising its potential to maximise environmental benefits and minimise scour. <i>Editor's note—Guidance on natural channel design principles can be found in the Council's publication Natural channel design guidelines.</i></p>	N/A	Not applicable.
	<p><b>AO8.3</b> Development provides stormwater outlets into waterways, creeks, wetlands and overland flow paths with energy dissipation to minimise scour in compliance with the standards in the Infrastructure design planning scheme policy.</p>	N/A	Not applicable.
	<p><b>AO8.4</b> Development ensures that the design of modifications to the existing design of new stormwater channels, creeks and major drains is in compliance with the standards in the Infrastructure design planning scheme policy.</p>	N/A	Not applicable.

<p><b>PO9</b> Development is designed to manage run-off and peak flows by minimising large areas of impervious material and maximising opportunities for capture and re-use.</p>	<p><b>AO9</b> No acceptable outcome is prescribed.</p>	A	Refer AO1 above.
<p><b>PO10</b> Development ensures that there is sufficient site area to accommodate an effective stormwater management system. <i>Note—Compliance with the performance outcome should be demonstrated by the submission of a site-based stormwater management plan for high-risk development only.</i></p>	<p><b>AO10</b> No acceptable outcome is prescribed.</p>	A	Refer AO1 above.
<p><b>PO11</b> Development provides for the orderly development of stormwater infrastructure within a catchment, having regard to the:</p> <ul style="list-style-type: none"> <li>(a) existing capacity of stormwater infrastructure within and external to the site, and any planned stormwater infrastructure upgrades;</li> <li>(b) safe management of stormwater discharge from existing and future up-slope development;</li> <li>(c) implication for adjacent and down-slope development.</li> </ul>	<p><b>AO11.1</b> Development with up-slope external catchment areas provides a drainage connection sized for ultimate catchment conditions that is directed to a lawful point of discharge.</p>	N/A	<p>Not applicable.</p> <p>No upstream drainage connection requirements have been identified for the development.</p>
	<p><b>AO11.2</b> Development ensures that existing stormwater infrastructure that is undersized is upgraded in compliance with the Priority infrastructure plan and the standards in the Infrastructure design planning scheme policy.</p>	N/A	Not applicable.
<p><b>PO12</b> Development provides stormwater infrastructure which:</p> <ul style="list-style-type: none"> <li>(a) remains fit for purpose for the life of the development and maintains full functionality in the design flood event;</li> <li>(b) can be safely accessed and maintained cost effectively;</li> <li>(c) ensures no structural damage to existing stormwater infrastructure.</li> </ul>	<p><b>AO12.1</b> The stormwater management system is designed in compliance with the Infrastructure design planning scheme policy.</p>	A	Refer AO1 above.
	<p><b>AO12.2</b> Development provides a clear area with a minimum of 2m radius from the centre of an existing manhole cover and with a minimum height clearance of 2.5m.</p>	N/A	Not applicable.

<p><b>PO13</b> Development ensures that all reasonable and practicable measures are taken to manage the impacts of erosion, turbidity and sedimentation, both within and external to the development site from construction activities, including vegetation clearing, earthworks, civil construction, installation of services, rehabilitation, revegetation and landscaping to protect:</p> <ul style="list-style-type: none"> <li>(a) the environmental values and water quality objectives of waters;</li> <li>(b) waterway hydrology;</li> <li>(c) the maintenance and serviceability of stormwater infrastructure.</li> </ul> <p><i>Note—The Infrastructure design planning scheme policy outlines the appropriate measures to be taken into account to achieve the performance outcome.</i></p>	<p><b>AO13</b> No acceptable outcome is prescribed.</p>	<p>A</p>	<p>An Erosion and Sediment Control Plan shall be prepared (prior to commencement of construction) and maintained for the management of sediment laden stormwater during the construction phase in accordance with the stormwater drainage section of the Infrastructure Design Planning Scheme Policy.</p>
<p><b>PO14</b> Development ensures that:</p> <ul style="list-style-type: none"> <li>(a) unnecessary disturbance to soil, waterways or drainage channels is avoided;</li> <li>(b) all soil surfaces remain effectively stabilised against erosion in the short and long term.</li> </ul>	<p><b>AO14</b> No acceptable outcome is prescribed.</p>	<p>N/A</p>	<p>Not applicable.</p>
<p><b>PO15</b> Development does not increase:</p> <ul style="list-style-type: none"> <li>(a) the concentration of total suspended solids or other contaminants in stormwater flows during site construction;</li> <li>(b) run-off which causes erosion either on site or off site.</li> </ul>	<p><b>AO15</b> No acceptable outcome is prescribed.</p>	<p>A</p>	<p>Contractor to ensure ESC measures are installed and maintained to avoid the release of contaminants from site.</p>

**Section B—Additional criteria which apply to high-risk development, being one or more of the following:**

- (a) a material change of use for an urban purpose which involves greater than 2,500m<sup>2</sup> of land that:
  - (iv) will result in an impervious area greater than 25% of the net developable area; or
  - (v) will result in 6 or more dwellings.
- (b) reconfiguring a lot for an urban purpose that involves greater than 2,500m<sup>2</sup> of land and will result in 6 or more lots;
- (c) operational work for an urban purpose which involves disturbing greater than 2,500m<sup>2</sup> of land.

<p><b>PO16</b></p> <p>Development ensures that the entry and transport of contaminants into stormwater is avoided or minimised to protect receiving water environmental values.</p> <p><i>Note—Prescribed water contaminants are defined in the Environmental Protection Act 1994.</i></p> <p><i>Note—Compliance with the performance outcome should be demonstrated by the submission of a site-based stormwater management plan for high-risk development only.</i></p>	<p><b>AO16</b></p> <p>Development provides a stormwater management system which is designed in compliance with the standards in the Infrastructure design planning scheme policy.</p>	<p>A/S</p>	<p>Please see the Site Based Stormwater Management Plan prepared by RCS.</p>
<p><b>PO17</b></p> <p>Development ensures that:</p> <ul style="list-style-type: none"> <li>(a) the discharge of wastewater to a waterway or external to the site is avoided; or</li> <li>(b) if the discharge cannot practicably be avoided, the development minimises wastewater discharge through re-use, recycling, recovery and treatment.</li> </ul>	<p><b>AO17</b></p> <p>No acceptable outcome is prescribed.</p>	<p>A/S</p>	<p>Sewer property connection is proposed for the development with consent for connections to the existing main within Kelliher Road</p>

**Section C—Additional performance outcomes and acceptable outcomes for assessable development for a material change of use or reconfiguring a lot**

<p><b>PO18</b></p> <p>Development protects stormwater infrastructure to ensure the following are not compromised:</p> <ul style="list-style-type: none"> <li>(a) the long term infrastructure for the stormwater network in the Long term infrastructure plans;</li> <li>(b) the existing and planned infrastructure for the stormwater network in the Local government infrastructure plan;</li> <li>(c) the provision of long term, existing and planned infrastructure for the stormwater network which:             <ul style="list-style-type: none"> <li>i. is required to service the development or an existing and future urban development in the planning scheme area; or</li> <li>ii. is in the interests of rational development or the efficient and orderly planning of the general area in which the site is situated.</li> </ul> </li> </ul> <p><i>Editor's note—A condition which requires a proposed development to keep permanent improvements and structures associated with the approved development clear of the area of long term infrastructure, may be imposed.</i></p>	<p><b>AO18</b></p> <p>Development protects stormwater infrastructure in compliance with the following:</p> <ul style="list-style-type: none"> <li>(a) for long term infrastructure for the stormwater network, the Long term infrastructure plans;</li> <li>(b) for existing and planned infrastructure for the stormwater network, the Local government infrastructure plan;</li> <li>(c) the standards for stormwater drainage in the Infrastructure design planning scheme policy.</li> </ul>	<p>N/A</p>	<p>Not applicable.</p>
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<p><b>PO19</b></p> <p>Development provides for the payment of extra trunk infrastructure costs for the following:</p> <ul style="list-style-type: none"> <li>(a) for development completely or partly outside the priority infrastructure area in the Local government infrastructure plan;</li> <li>(b) for development completely inside the priority infrastructure area in the Local government infrastructure plan involving: <ul style="list-style-type: none"> <li>i. trunk infrastructure that is to be provided earlier than planned in the Local government infrastructure plan;</li> <li>ii. long term infrastructure for the stormwater network which is made necessary by development that is not assumed future urban development;</li> <li>iii. other infrastructure for the stormwater network associated with development that is not assumed future urban development which is made necessary by the development.</li> </ul> </li> </ul> <p><i>Editor's note—The payment of extra trunk infrastructure costs for development completely inside the priority infrastructure area in the Local government infrastructure plan is to be worked out in accordance with the Charges Resolution.</i></p> <p><i>Editor's note—See section 130 Imposing Development conditions (Conditions for extra trunk infrastructure costs) of the Planning Act 2016.</i></p>	<p><b>AO19</b></p> <p>No acceptable outcome is prescribed.</p>	<p>N/A</p>	<p>Not applicable.</p>
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**Solution Legend**

- A = Acceptable solution
- A/S = Alternate solution proposed
- N/A = Not applicable to this application

## 4. Appendices

- 4.1 Appendix A – Proposed Development Plans
- 4.2 Appendix B – Detailed Survey
- 4.3 Appendix C – Concept Engineering Plans
- 4.4 Appendix D – BCC Property Lot Report
- 4.5 Appendix E – Erosion Hazard Assessment
- 4.6 Appendix F – Floodwise Property Report



APPENDIX A  
PROPOSED DEVELOPMENT PLANS

# RCS

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CONSULTANTS  
YOU CAN TRUST



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Email: [adrian@rcs.net.au](mailto:adrian@rcs.net.au)  
Mob: 0401338071



**Legend**

	EASEMENT
	LANDSCAPE RECLAMATION ZONE
	RETAINED TREE CANOPY COVER
	RETAINED TREE ROOT PROTECTION ZONE
	ROAD WIDENING
	TREE ROOT ENCROACHMENT AREA

**A** SITE PLAN  
1: 750 @ A3



APPENDIX B  
PROPOSED DETAILED SURVEY

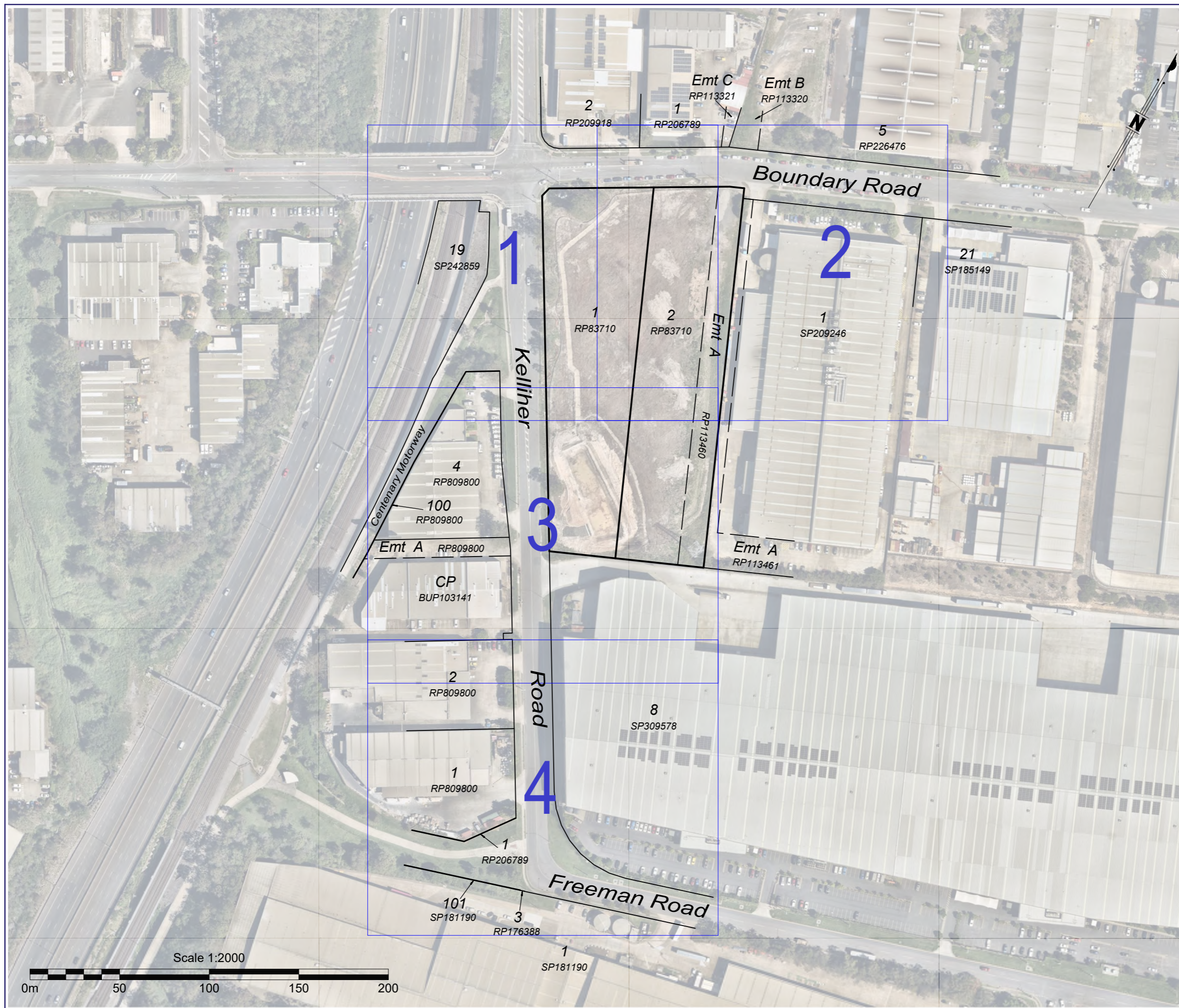
# RCS

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Mob: 0401338071



**LEGEND:**

<b>Road</b>	Kerb Back	Terrain	Driveway
	Kerb Invert		Footpath
	Edge of Bitumen		Slab Edge
	Edge of Gravel		Retaining Wall Base
	Road Crown		Retaining Wall Top
	Traffic Park Meter		Change of Grade
	Traffic Light		Top of Bank
	Traffic Pit		Toe of Bank
	Traffic Sign		Waterline
	Traffic Post Box		Garden Edge
<b>Structures</b>	Building Line		Creek Toe of Bank
	Roof Ridgeline		Creek Top of Bank
	Fence Line		Creek Invert
	Gate		Creek Waterline
	Hand Rail		Tree
	Cattle Grid		Tree Canopy
	Bollard		Control Point/PSM
<b>Sewer</b>	Sewer Line	<b>Drainage</b>	Drainage line
	Valve		Open Drain
	Inspection Opening		Manhole
	Manhole		Field Inlet
			Downpipe
<b>Electrical</b>	Electricity Line	<b>Communications</b>	Communication Line
	Overhead Line		Overhead Line
	Pits/Manholes		Pits/Manholes
	Pole		Pillar
	Street Light		
	Light In-Ground		
<b>Water</b>	Water Line	<b>Gas</b>	Gas Line
	Meter		Valve
	Valve		Marker
	Fire Hydrant		
	Tap		
	Sprinkler	<b>General</b>	Pothole
<b>Fuel</b>	Fuel Line	<b>Subsurface Utility - QL</b>	
	Fitting	* (A)	* QL-A (H±50mm, V±50mm)
		* (B)	* QL-B (H±300mm, V±500mm)
		* (C)	* QL-C (H±300mm, 2D)
		* (D)	* QL-D (Exist Record)

**NOTES:**

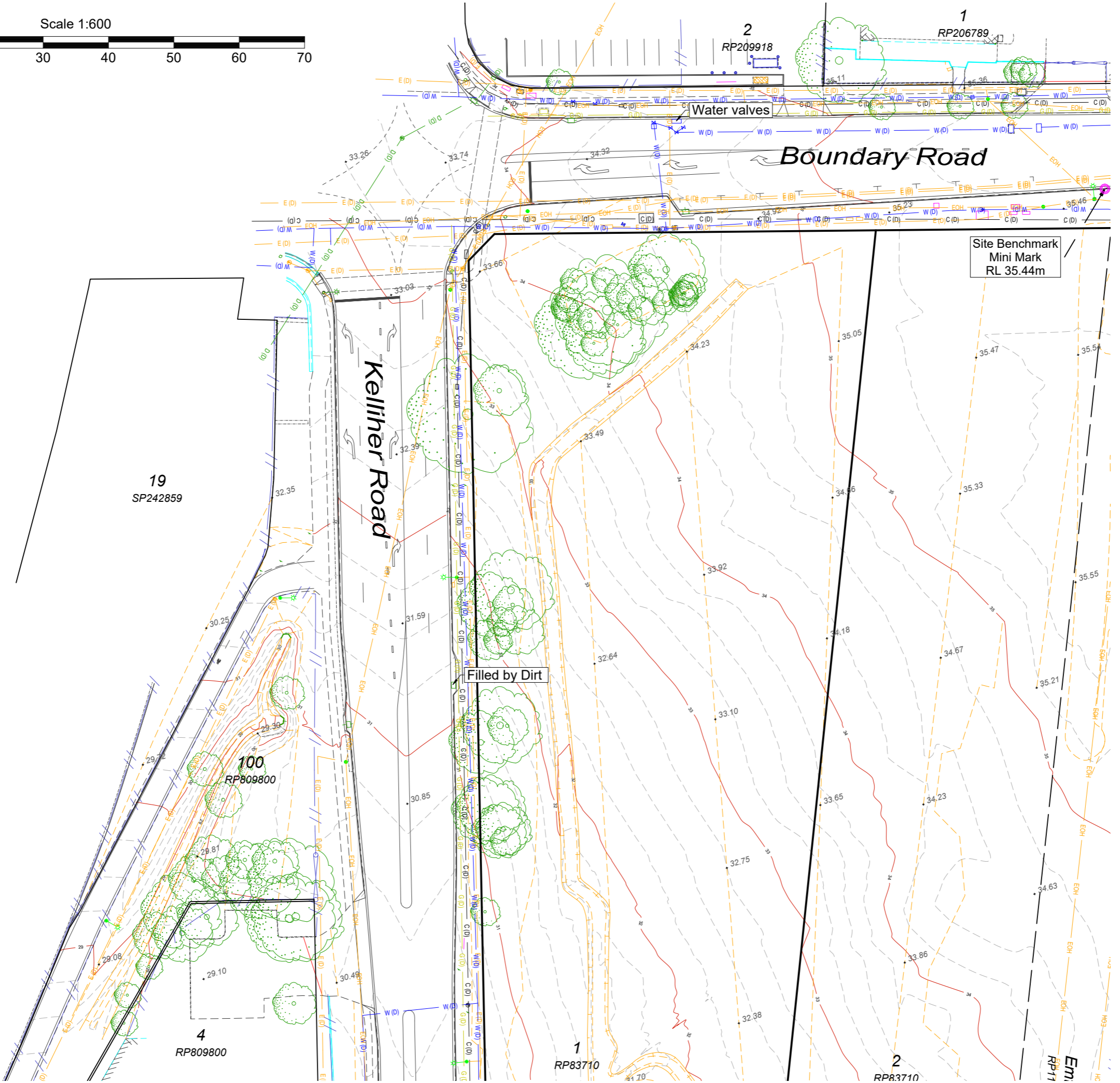
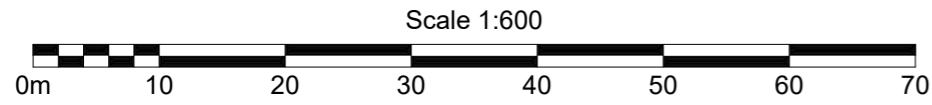
1. Drawn to scale on an A3 sheet.
2. Contour Interval... 0.25m
3. All levels are in metres on the Australian Height Datum referred to PSM125554 - RL 34.465 AHD situated in Boundary Rd, Richlands.
4. Subject lot boundaries are surveyed vide identification survey IS343777. Adjoining boundaries are vide title and subject to confirmation by survey. No volumetric surveys shown on this survey.
5. The Location of Underground services are in accordance with AS5488:2019, the Australian Standard for classification of Subsurface Utility Information (SUI). The exact nature and location of these services should be confirmed prior to construction.
6. Field Survey Completed on 2/02/2026.

Level datum: AHD Derived (PSM125554)  
Horiz datum: MGA Derived GNSS (PSM72024)  
Coord Origin: GNSS (9003DPY)  
GDA System: GDA2020 Coordinate System: Plane 1:1  
Meridian: GNSS

Title:

**VERIFICATION PLOT**  
Lot 1 & 2 on RP83710  
804 & 816 Boundary Rd  
Richlands, QLD

Client:	<b>Goodhew Construction Services</b>
Locality:	Richlands
Local Gov:	Brisbane City Council
Surveyed By:	SBA
Approved:	CTS
Date Created:	18/02/26
Scale:	1:2000
File Ref:	13275
Plan No:	<b>13275_S004_VER</b> Rev: <b>A</b>



**LEGEND:**

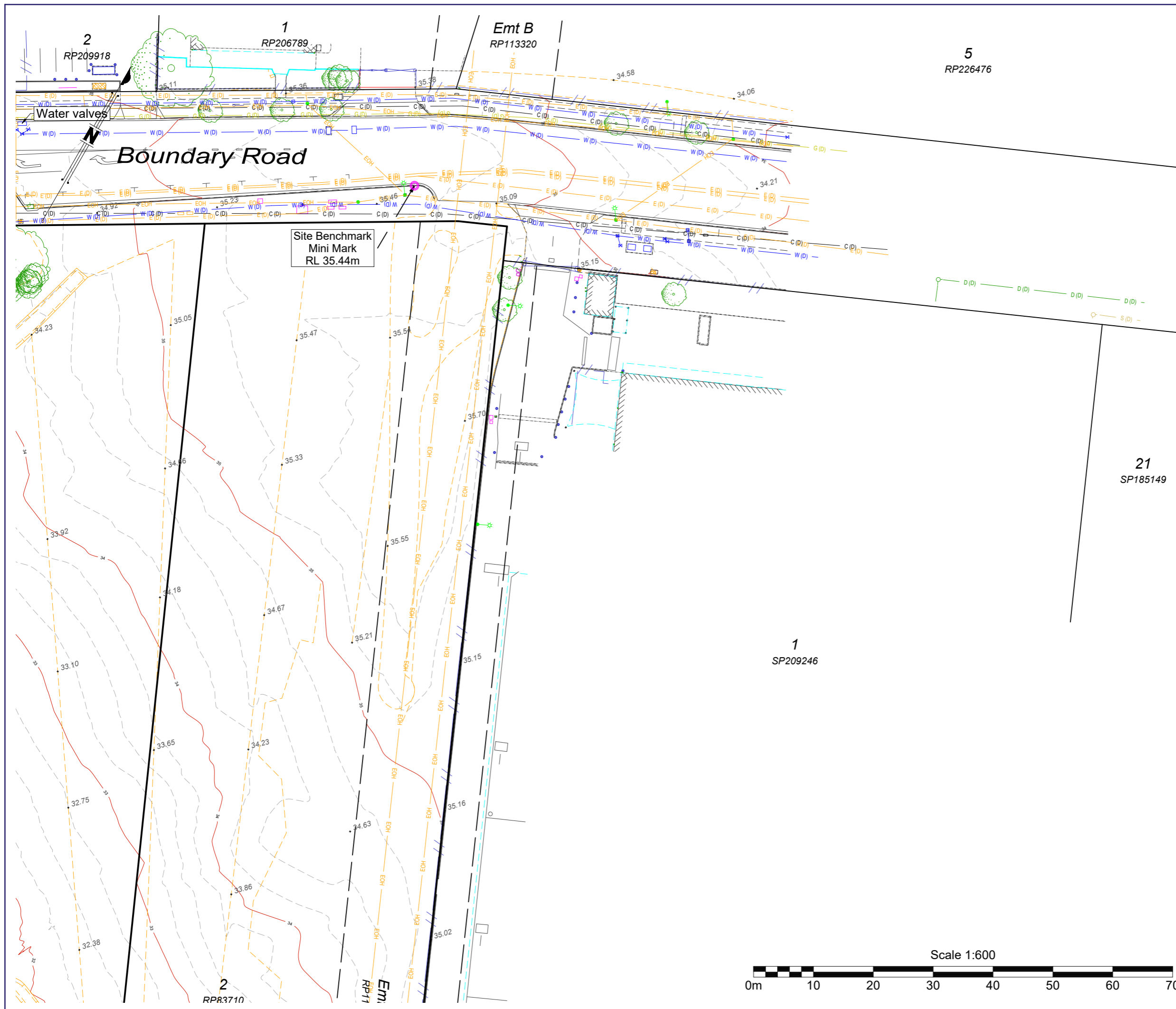
<b>Road</b>	<b>Terrain</b>
Kerb Back	Driveway
Kerb Invert	Footpath
Edge of Bitumen	Slab Edge
Edge of Gravel	Retaining Wall Base
Road Crown	Retaining Wall Top
Traffic Park Meter	Change of Grade
Traffic Light	Top of Bank
Traffic Pit	Toe of Bank
Traffic Sign	Waterline
Traffic Post Box	Garden Edge
<b>Structures</b>	Creek Toe of Bank
Building Line	Creek Top of Bank
Roof Ridgeline	Creek Invert
Fence Line	Creek Waterline
Gate	Tree
Hand Rail	Tree Canopy
Cattle Grid	Control Point/PSM
Bollard	
<b>Sewer</b>	<b>Drainage</b>
Sewer Line	Drainage line
Valve	Open Drain
Inspection Opening	Manhole
Manhole	Field Inlet
<b>Electrical</b>	<b>Communications</b>
Electricity Line	Communication Line
Overhead Line	Overhead Line
Pits/Manholes	Pits/Manholes
Pole	Pillar
Street Light	
Light In-Ground	<b>Gas</b>
<b>Water</b>	Gas Line
Water Line	Valve
Meter	Marker
Valve	
Fire Hydrant	<b>General</b>
Tap	Pothole
Sprinkler	
<b>Fuel</b>	<b>Subsurface Utility - QL</b>
Fuel Line	* QL-A (H±50mm, V±50mm)
Fitting	* QL-B (H±300mm, V±500mm)
	* QL-C (H±300mm, 2D)
	* QL-D (Exist Record)

- NOTES:**
1. Drawn to scale on an A3 sheet.
  2. Contour Interval... 0.25m
  3. All levels are in metres on the Australian Height Datum referred to PSM125554 - RL 34.465 AHD situated in Boundary Rd, Richlands.
  4. Subject lot boundaries are surveyed vide identification survey IS343777. Adjoining boundaries are vide title and subject to confirmation by survey. No volumetric surveys shown on this survey.
  5. The Location of Underground services are in accordance with AS5488:2019, the Australian Standard for classification of Subsurface Utility Information (SUI). The exact nature and location of these services should be confirmed prior to construction.
  6. Field Survey Completed on 2/02/2026.

Level datum: AHD Derived (PSM125554)  
 Horiz datum: MGA Derived GNSS (PSM72024)  
 Coord Origin: GNSS (9003DPY)  
 GDA System: GDA2020 Coordinate System: Plane 1:1  
 Meridian: GNSS

**Title:**  
**VERIFICATION PLOT**  
 Lot 1 & 2 on RP83710  
 804 & 816 Boundary Rd  
 Richlands

Client:	<b>Goodhew Construction Services</b>
Locality:	Richlands
Local Gov:	Brisbane City Council
Surveyed By:	SBA
Approved:	CTS
Date Created:	18/02/26
Scale:	1:600
File Ref:	13275
Plan No:	<b>13275_S004_VER</b> Rev: <b>A</b>



**LEGEND:**

<b>Road</b>	<b>Terrain</b>
Kerb Back	Driveway
Kerb Invert	Footpath
Edge of Bitumen	Slab Edge
Edge of Gravel	Retaining Wall Base
Road Crown	Retaining Wall Top
Traffic Park Meter	Change of Grade
Traffic Light	Top of Bank
Traffic Pit	Toe of Bank
Traffic Sign	Waterline
Traffic Post Box	Garden Edge
<b>Structures</b>	Creek Toe of Bank
Building Line	Creek Top of Bank
Roof Ridgeline	Creek Invert
Fence Line	Creek Waterline
Gate	Tree
Hand Rail	Tree Canopy
Cattle Grid	Control Point/PSM
Bollard	
<b>Sewer</b>	<b>Drainage</b>
Sewer Line	Drainage line
Valve	Open Drain
Inspection Opening	Manhole
Manhole	Field Inlet
<b>Electrical</b>	Downpipe
Electricity Line	Communications
Overhead Line	Communication Line
Pits/Manholes	Overhead Line
Pole	Pits/Manholes
Street Light	Pillar
Light In-Ground	
<b>Water</b>	<b>Gas</b>
Water Line	Gas Line
Water Meter	Valve
Valve	Marker
Fire Hydrant	
Tap	<b>General</b>
Sprinkler	Pothole
<b>Fuel</b>	<b>Subsurface Utility - QL</b>
Fuel Line	* (A) * QL-A (H±50mm, V±50mm)
Fitting	* (B) * QL-B (H±300mm, V±500mm)
	* (C) * QL-C (H±300mm, 2D)
	* (D) * QL-D (Exist Record)

**NOTES:**

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GDA System: GDA2020 Coordinate System: Plane 1:1  
Meridian: GNSS

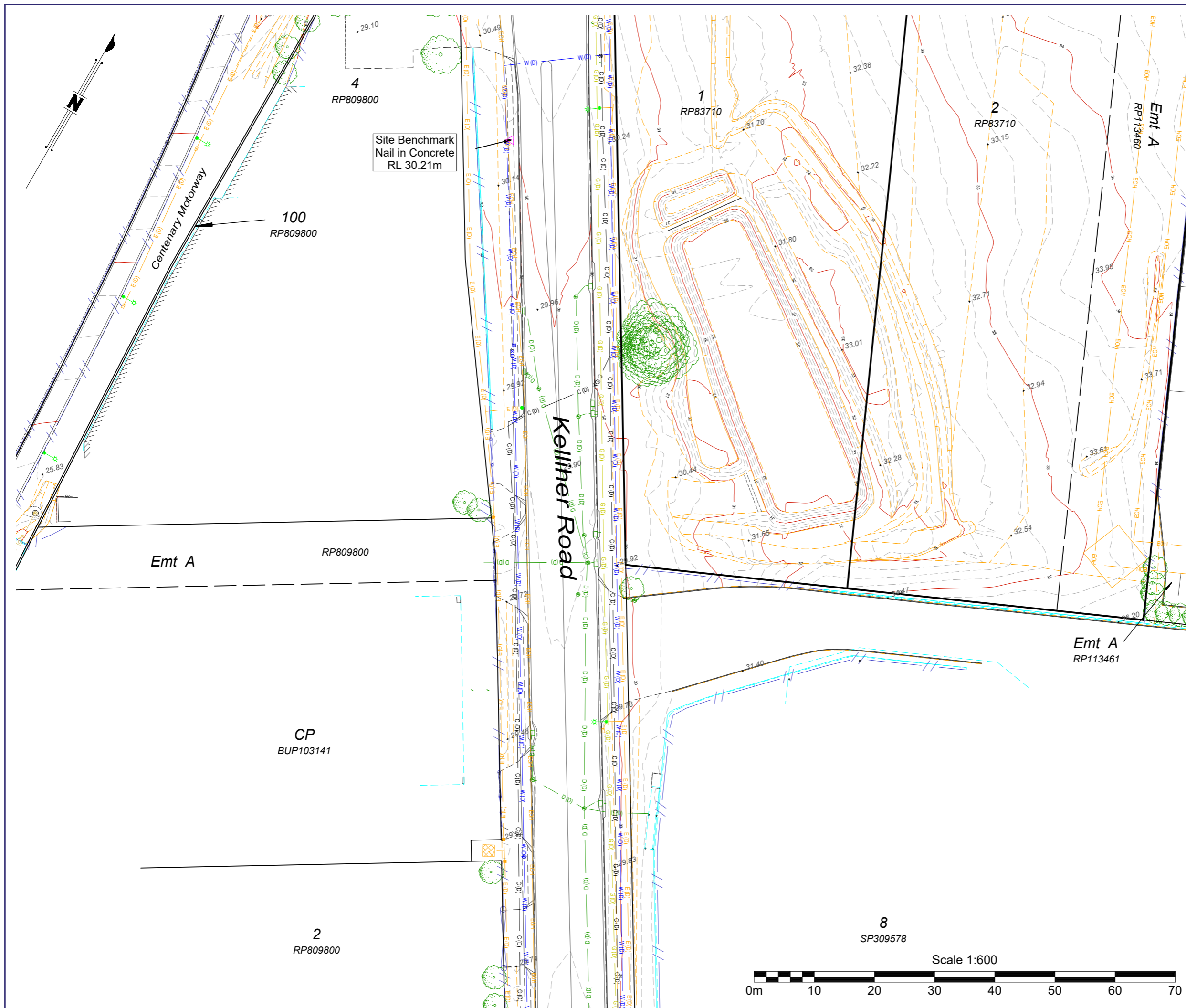
Title:

**VERIFICATION PLOT**  
Lot 1 & 2 on RP83710  
804 & 816 Boundary Rd  
Richlands

Client:	<b>Goodhew Construction Services</b>
Locality:	Richlands
Local Gov:	Brisbane City Council
Surveyed By:	SBA
Approved:	CTS
Date Created:	18/02/26
Scale:	1:600
File Ref:	13275
Plan No:	<b>13275_S004_VER</b> Rev: <b>A</b>

Scale 1:600





**LEGEND:**

<b>Road</b>	Kerb Back	<b>Terrain</b>	Driveway
	Kerb Invert		Footpath
	Edge of Bitumen		Slab Edge
	Edge of Gravel		Retaining Wall Base
	Road Crown		Retaining Wall Top
	Traffic Park Meter		Change of Grade
	Traffic Light		Top of Bank
	Traffic Pit		Toe of Bank
	Traffic Sign		Waterline
	Traffic Post Box		Garden Edge
<b>Structures</b>	Building Line		Creek Toe of Bank
	Roof Ridgeline		Creek Top of Bank
	Fence Line		Creek Invert
	Gate		Creek Waterline
	Hand Rail		Tree
	Cattle Grid		Tree Canopy
	Bollard		Control Point/PSM
<b>Sewer</b>	Sewer Line	<b>Drainage</b>	Drainage line
	Valve		Open Drain
	Inspection Opening		Manhole
	Manhole		Field Inlet
			Downpipe
<b>Electrical</b>	Electricity Line	<b>Communications</b>	Communication Line
	Overhead Line		Overhead Line
	Pits/Manholes		Pits/Manholes
	Pole		Pillar
	Street Light	<b>Gas</b>	Gas Line
	Light In-Ground		Valve
<b>Water</b>	Water Line		Marker
	Meter	<b>General</b>	Pothole
	Valve		
	Fire Hydrant	<b>Subsurface Utility - QL</b>	
	Tap		
	Sprinkler		
<b>Fuel</b>	Fuel Line		
	Fitting		

**NOTES:**

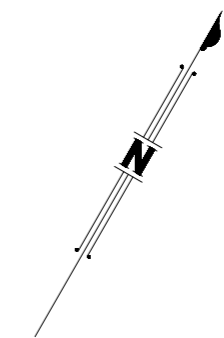
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Horiz datum: MGA Derived GNSS (PSM72024)  
Coord Origin: GNSS (9003DPY)  
GDA System: GDA2020 Coordinate System: Plane 1:1  
Meridian: GNSS

Title:

**VERIFICATION PLOT**  
Lot 1 & 2 on RP83710  
804 & 816 Boundary Rd  
Richlands

Client:	<b>Goodhew Construction Services</b>
Locality:	Richlands
Local Gov:	Brisbane City Council
Surveyed By:	SBA Approved: CTS
Date Created:	18/02/26 Scale: 1:600
File Ref:	13275
Plan No:	<b>13275_S004_VER</b> Rev: <b>A</b>



**LEGEND:**

Road	Terrain
Kerb Back	Driveway
Kerb Invert	Footpath
Edge of Bitumen	Slab Edge
Edge of Gravel	Retaining Wall Base
Road Crown	Retaining Wall Top
Traffic Park Meter	Change of Grade
Traffic Light	Top of Bank
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Gate	Tree
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Cattle Grid	Control Point/PSM
Bollard	Drainage
Sewer	Drainage line
Sewer Line	Open Drain
Valve	Manhole
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Manhole	Downpipe
Electrical	Communications
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Light In-Ground	Gas Line
Water	Valve
Water Line	Marker
Meter	General
Valve	Pothole
Fire Hydrant	Subsurface Utility - QL
Tap	* (A) * QL-A (H±50mm, V±50mm)
Sprinkler	* (B) * QL-B (H±300mm, V±500mm)
Fuel	* (C) * QL-C (H±300mm, 2D)
Fuel Line	* (D) * QL-D (Exist Record)
Fitting	

**NOTES:**

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Coord Origin: GNSS (9003DPY)  
GDA System: GDA2020 Coordinate System: Plane 1:1  
Meridian: GNSS

Title:

**VERIFICATION PLOT**  
Lot 1 & 2 on RP83710  
804 & 816 Boundary Rd  
Richlands

Client: **Goodhew Construction Services**

Locality: Richlands

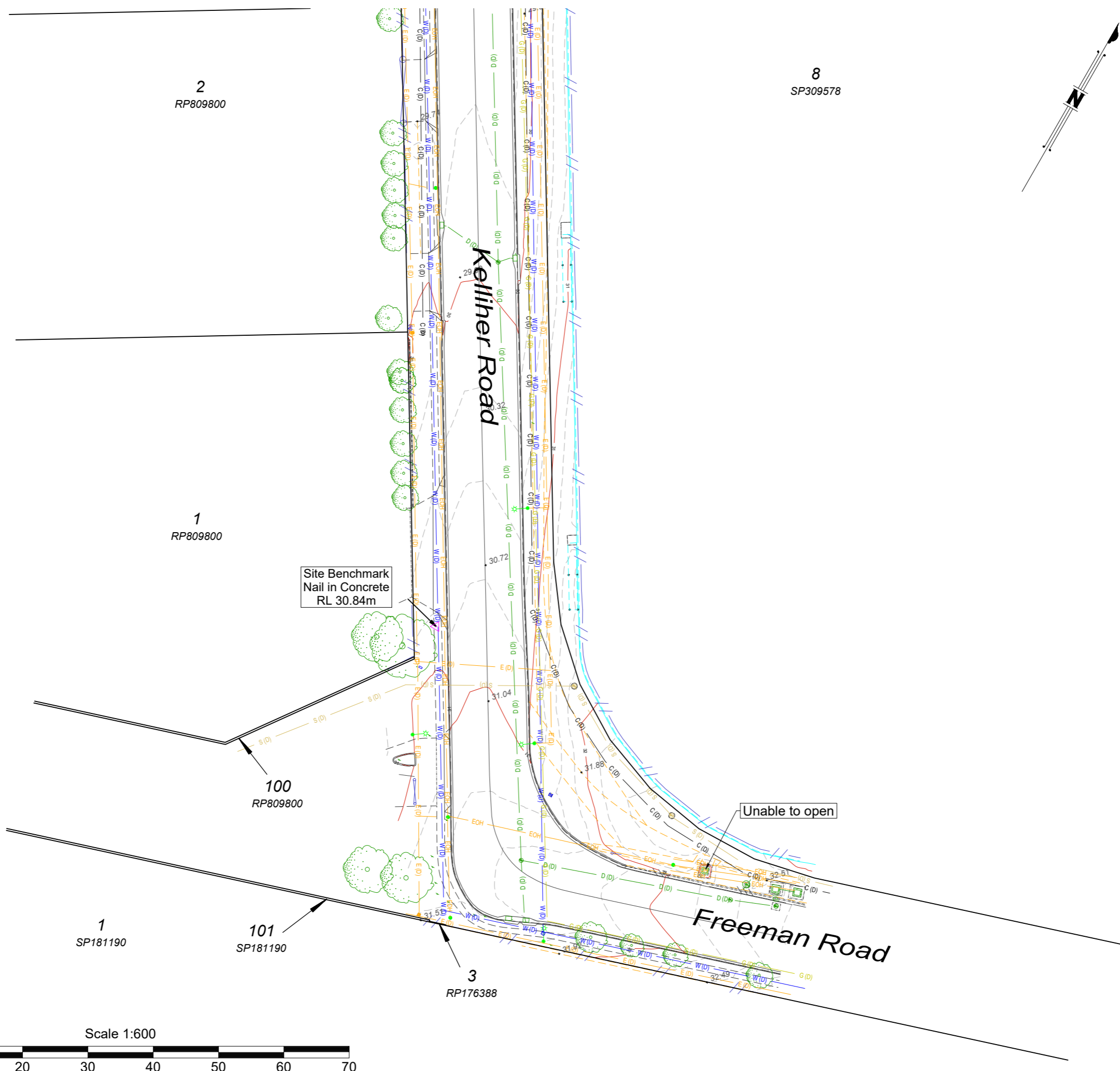
Local Gov: Brisbane City Council

Surveyed By: SBA Approved: CTS

Date Created: 18/02/26 Scale: 1:600

File Ref: 13275

Plan No: **13275\_S004\_VER** Rev: **A**



2  
RP809800

8  
SP309578

1  
RP809800

Site Benchmark  
Nail in Concrete  
RL 30.84m

100  
RP809800

Unable to open

1  
SP181190

101  
SP181190

3  
RP176388

Freeman Road

Scale 1:600



APPENDIX C  
CONCEPT ENGINEERING PLANS

**RCS**

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CONSULTANTS  
YOU CAN TRUST

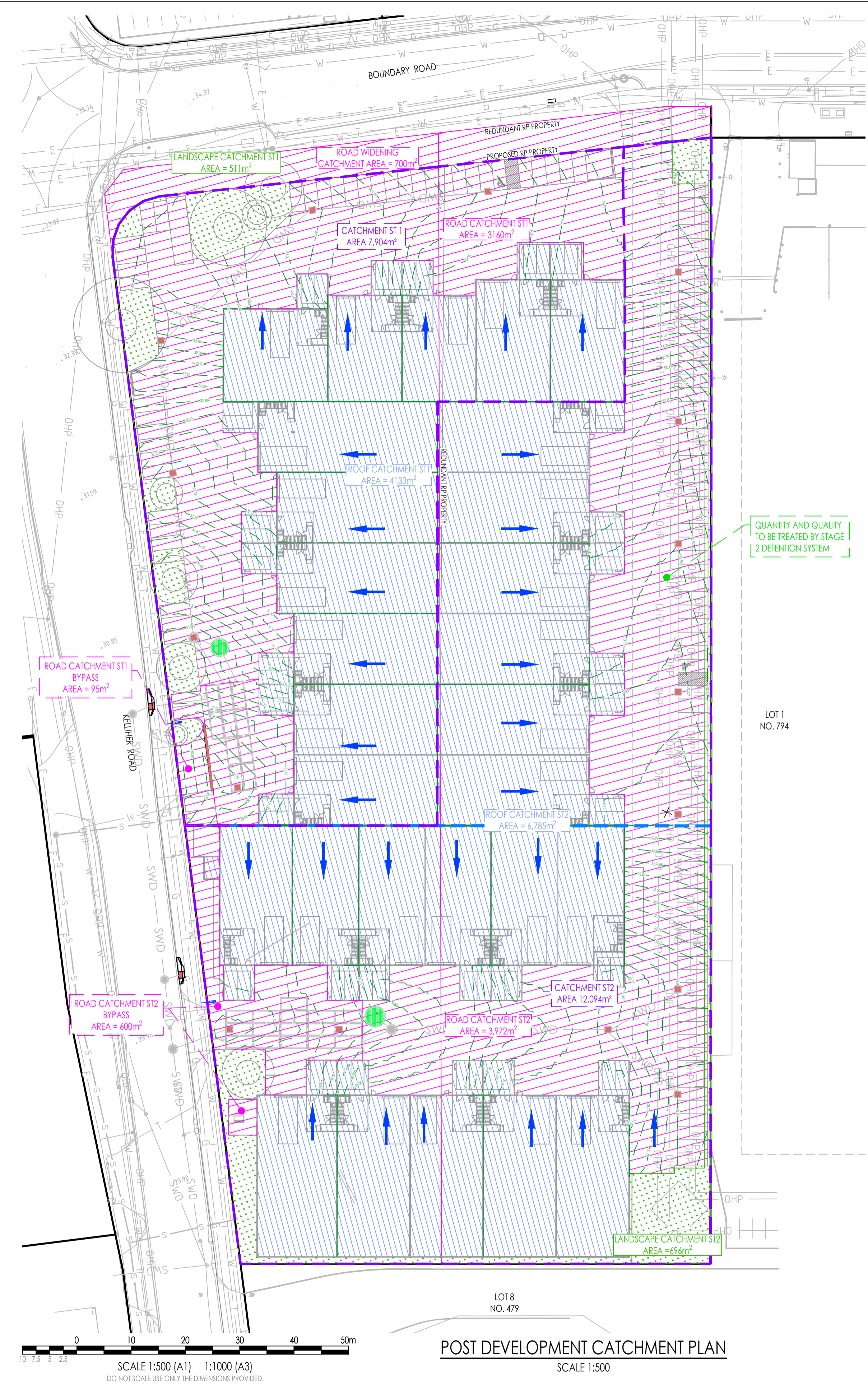


LOCATION AND LEVELS OF ALL EXISTING SERVICES AND PROPOSED STORMWATER OUTLETS TO BE CONFIRMED ON SITE BY THE CONTRACTOR PRIOR TO THE COMMENCEMENT OF WORKS. IT IS THE CONTRACTOR'S RESPONSIBILITY TO PERFORM A 'BEFORE YOU DIG' SEARCH AND CARRY OUT ALL INVESTIGATIONS REQUIRED TO SATISFY THEMSELVES OF THE LOCATION OF EXISTING SERVICES. ANY POTENTIAL CONFLICTS SHALL BE REPORTED TO THE SUPERINTENDENT.

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THE ABOVE ALSO APPLIES TO LOCATION AND DEPTH OF OTHER PUBLIC UTILITIES.



PRE DEVELOPMENT CATCHMENT PLAN  
SCALE 1:500



POST DEVELOPMENT CATCHMENT PLAN  
SCALE 1:500

QUANTITY AND QUALITY TO BE TREATED BY STAGE 2 DETENTION SYSTEM

LEGEND	
	EXISTING SURFACE CONTOURS
	FINISHED SURFACE CONTOURS
	PROPOSED LOT BOUNDARY
	REDUNDANT LOT BOUNDARY
	EXISTING LOT BOUNDARY
	EXISTING STORMWATER DRAINAGE
	EXISTING SEWER RETICULATION
	EXISTING WATER RETICULATION
	EXISTING UNDERGROUND POWER
	EXISTING OVERHEAD POWER
	EXISTING TELSTRA LINE
	EXISTING NBN LINE
	EXISTING GAS LINE
	STORMWATER CATCHMENT AREA
	STAGING BOUNDARY
	IMPERVIOUS CATCHMENT AREA
	ROAD CATCHMENT AREA
	LANDSCAPING CATCHMENT AREA
	ROOF CATCHMENT AREA
	STORMWATER FLOW ARROWS

AMENDMENTS				
NO.	DATE	DESCRIPTION	DRAWN	
A	11.05.2026	PRELIMINARY ISSUE	KN	



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**IMAGERY, CADASTRAL AND SURVEY DATA DISCLAIMER**  
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NORTH:

RP DESCRIPTION:  
LOT 1 & 2 ON RP83710

CLIENT:  
ALSAPCE PROPERTY GROUP UNIT TRUST

SCALE: 1 : 500      ORIG. DRG. SIZE A1

APPROVED:   
On behalf of Rodgers Consulting Services

DRAWN: KN      DESIGN: AR

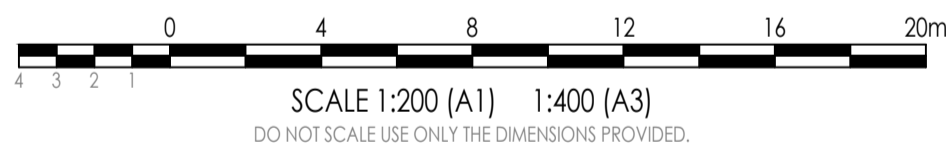
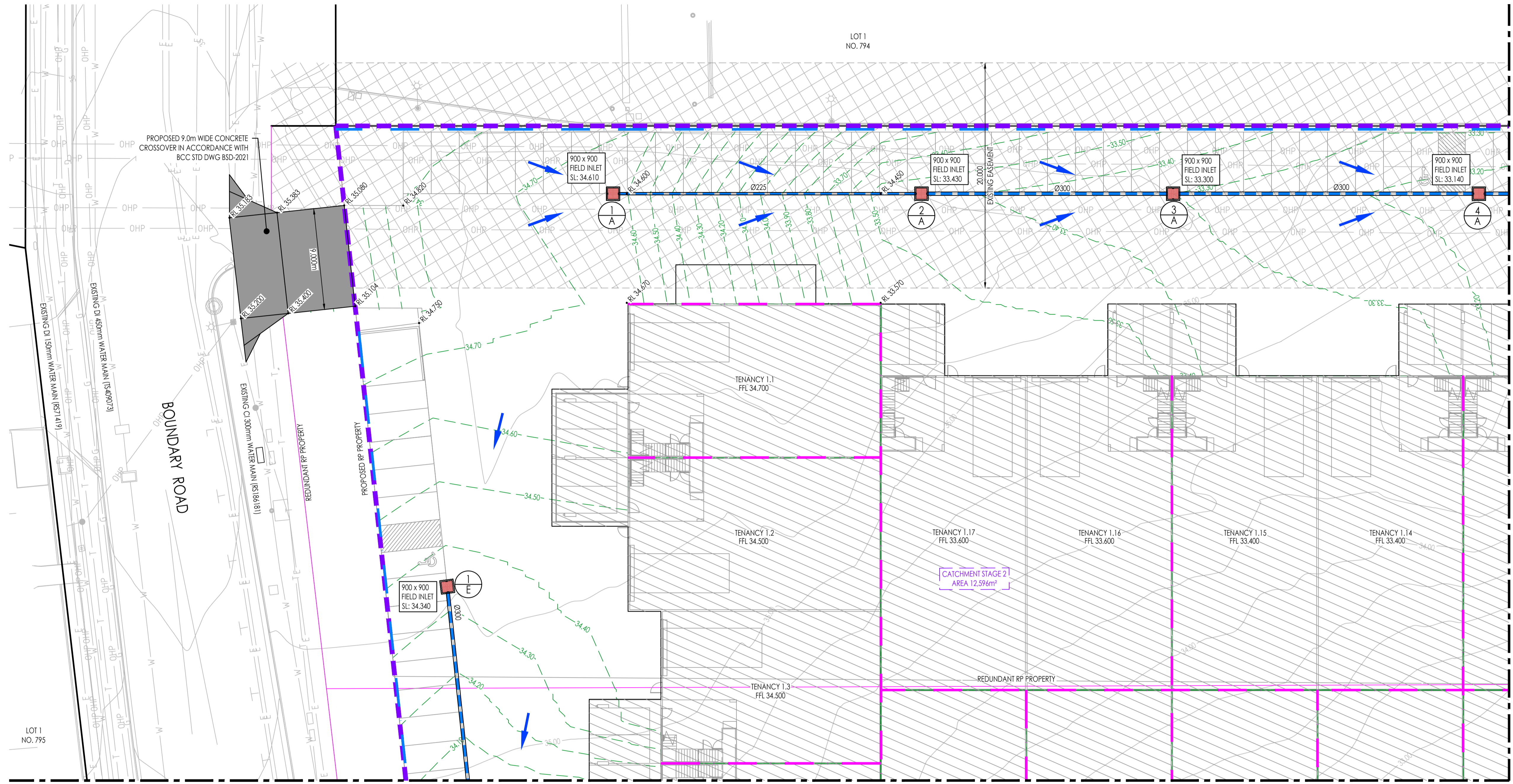
CHECKED: TS      SUPERVISOR: AR

RPEQ.      DATE:

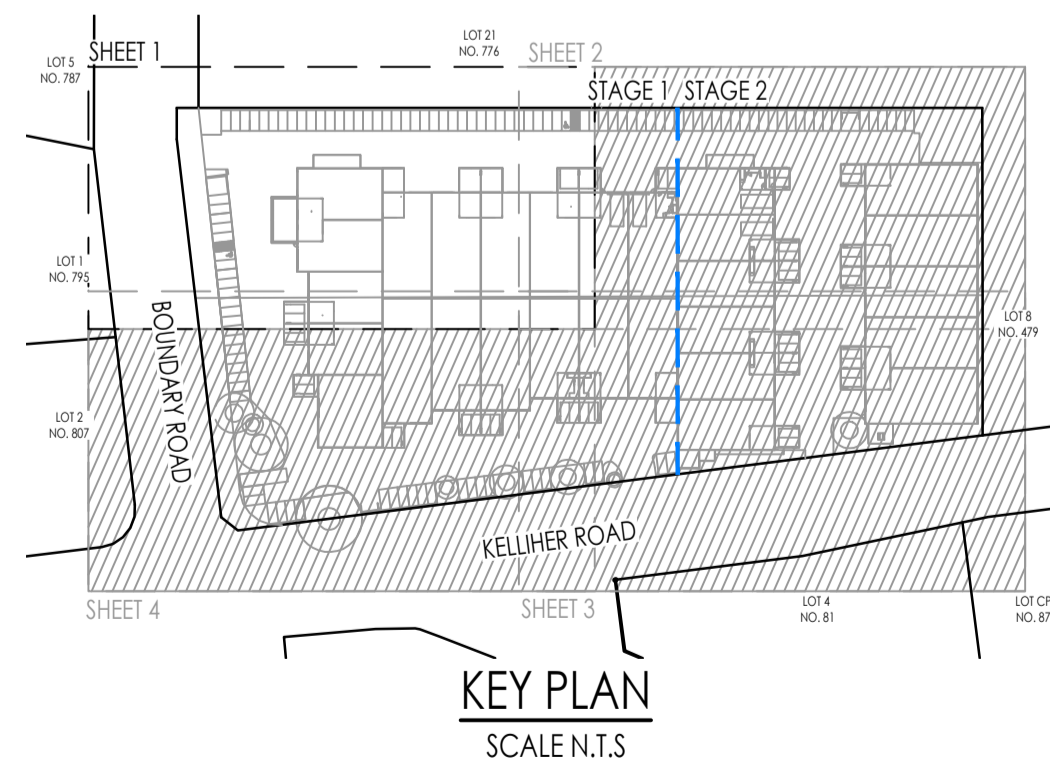
PROJECT:  
804 - 816 BOUNDARY ROAD, RICHLANDS QLD 4077

TITLE:  
PRE & POST STORMWATER CATCHMENT LAYOUT PLAN

APPROVAL	
JOB NO.	C26011
DRAWING NO.	SK01
REVISION:	A



CONCEPT STORMWATER DRAINAGE LAYOUT PLAN SHEET 1 OF 4  
SCALE 1:200



LEGEND

- - - 5.50 - - - EXISTING SURFACE CONTOURS
- - - 5.50 - - - FINISHED SURFACE CONTOURS
- PROPOSED LOT BOUNDARY
- REDUNDANT LOT BOUNDARY
- EXISTING LOT BOUNDARY
- SWD — EXISTING STORMWATER DRAINAGE
- S — EXISTING SEWER RETICULATION
- W — EXISTING WATER RETICULATION
- E — EXISTING UNDERGROUND POWER
- OHP — EXISTING OVERHEAD POWER
- T — EXISTING TELSTRA LINE
- NBN — EXISTING NBN LINE
- G — EXISTING GAS LINE
- PROPOSED STORMWATER DRAINAGE
- 1 AE — STORMWATER STRUCTURE NUMBER
- STORMWATER CATCHMENT AREA
- STAGING BOUNDARY
- STORMWATER FIELD INLET
- STORMWATER MANHOLE
- STORMWATER CATCHPITS
- STORMWATER FLOW ARROWS
- RETAINING WALL
- BUILDING RETAINING WALL
- DRIVEWAY CROSSOVER HATCH
- EASEMENT

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**BEFORE YOU DIG**  
www.byda.com.au

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NORTH:

RP DESCRIPTION:  
LOT 1 & 2 ON RP83710

CLIENT:  
ALSAPCE PROPERTY GROUP UNIT TRUST

SCALE: 1 : 200

ORIG. DRG. SIZE  
A1

APPROVED:  
On behalf of Rodgers Consulting Services  
RPEQ. DATE:

DRAWN:  
KN

DESIGN:  
AR

CHECKED:  
TS

SUPERVISOR:  
AR

PROJECT:  
804 - 816 BOUNDARY ROAD, RICHLANDS QLD 4077

TITLE:  
CONCEPT STORMWATER DRAINAGE LAYOUT PLAN SHEET 1 OF 4

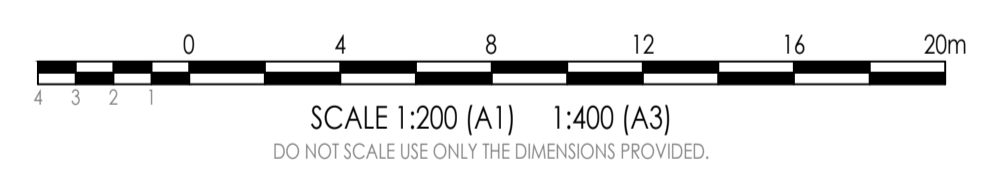
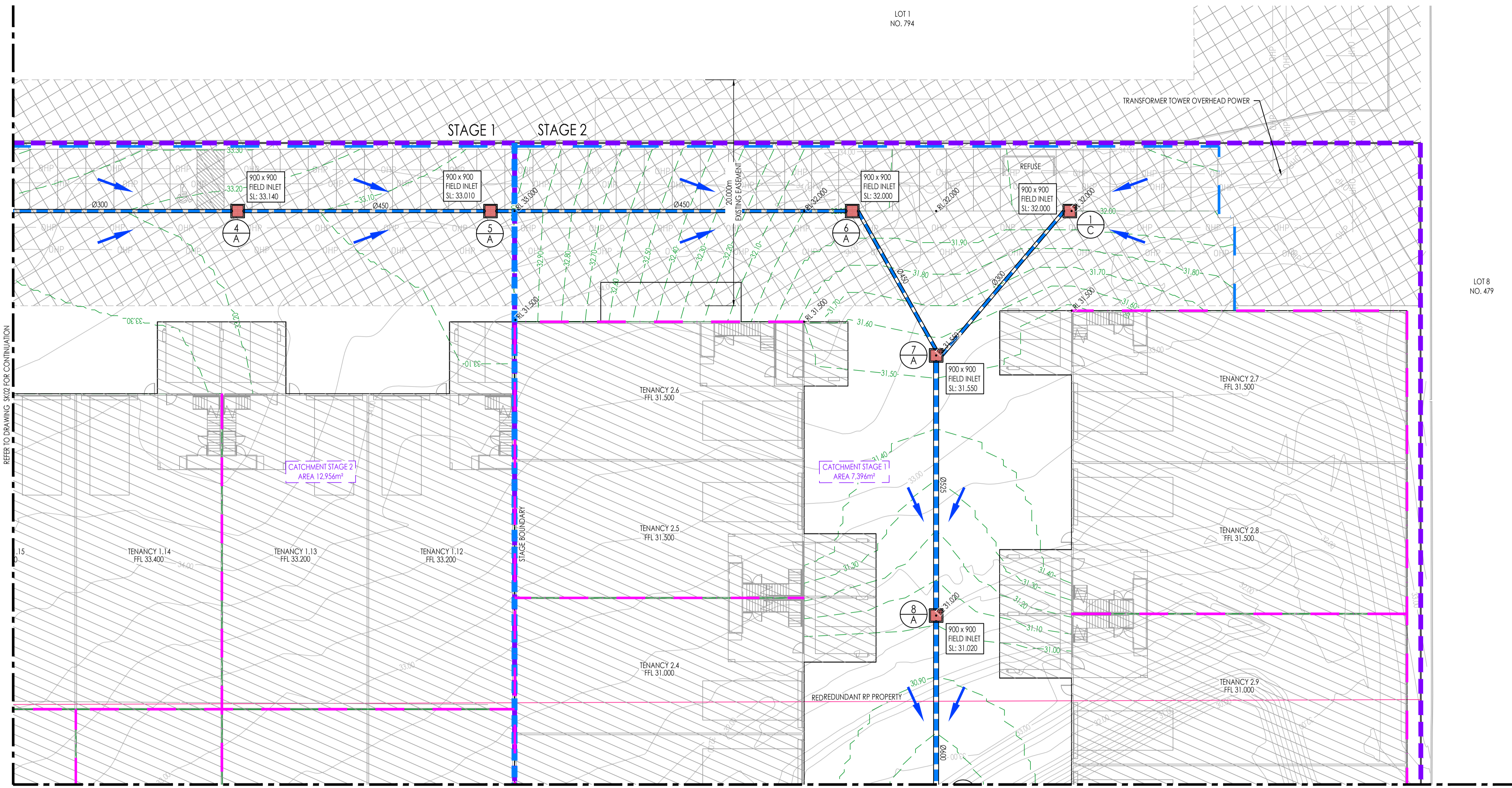
**APPROVAL**

JOB NO.  
C26011

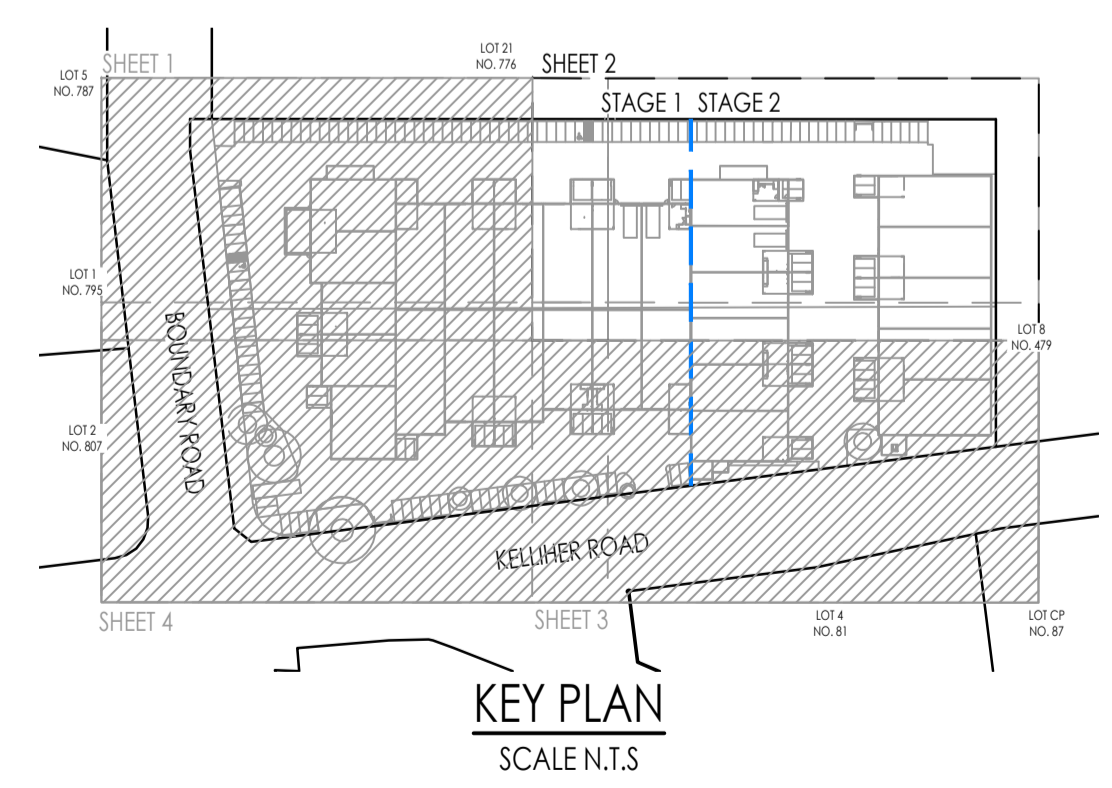
DRAWING NO.  
SK02

REVISION:  
A





CONCEPT STORMWATER DRAINAGE LAYOUT PLAN SHEET 2 OF 4  
SCALE 1:200



**LEGEND**

---	EXISTING SURFACE CONTOURS	---	EXISTING SURFACE CONTOURS
---	FINISHED SURFACE CONTOURS	---	FINISHED SURFACE CONTOURS
---	PROPOSED LOT BOUNDARY	---	PROPOSED LOT BOUNDARY
---	REDUNDANT LOT BOUNDARY	---	REDUNDANT LOT BOUNDARY
---	EXISTING LOT BOUNDARY	---	EXISTING LOT BOUNDARY
---	SWD	---	EXISTING STORMWATER DRAINAGE
---	S	---	EXISTING SEWER RETICULATION
---	W	---	EXISTING WATER RETICULATION
---	E	---	EXISTING UNDERGROUND POWER
---	OHP	---	EXISTING OVERHEAD POWER
---	T	---	EXISTING TELSTRA LINE
---	NBN	---	EXISTING NBN LINE
---	G	---	EXISTING GAS LINE
---	---	---	PROPOSED STORMWATER DRAINAGE
---	---	---	STORMWATER STRUCTURE NUMBER
---	---	---	STORMWATER CATCHMENT AREA
---	---	---	STAGING BOUNDARY
---	---	---	STORMWATER FIELD INLET
---	---	---	STORMWATER MANHOLE
---	---	---	STORMWATER CATCHPITS
---	---	---	STORMWATER FLOW ARROWS
---	---	---	RETAINING WALL
---	---	---	BUILDING RETAINING WALL
---	---	---	DRIVEWAY CROSSOVER HATCH
---	---	---	EASEMENT

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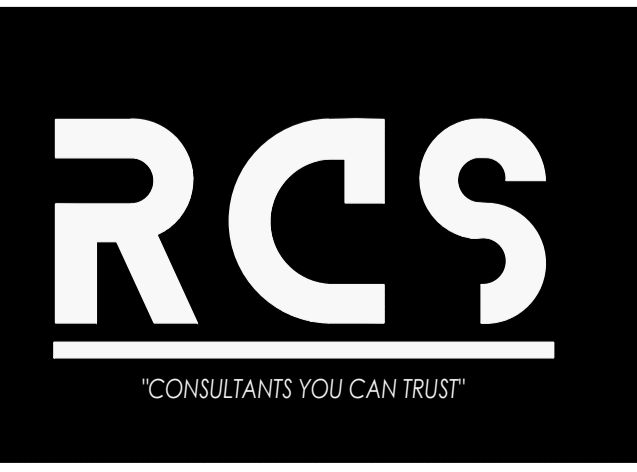
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AMENDMENTS

NO.	DATE	DESCRIPTION	DRAWN
A	11.05.2026	PRELIMINARY ISSUE	KN



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NORTH:

RP DESCRIPTION:  
LOT 1 & 2 ON RP83710

CLIENT:  
ALSAPCE PROPERTY GROUP UNIT TRUST

SCALE: 1 : 100

ORIG. DRG. SIZE  
A1

APPROVED:  
On behalf of Rodgers Consulting Services  
RPEQ. DATE:

DRAWN:  
KN

DESIGN:  
AR

CHECKED:  
TS

SUPERVISOR:  
AR

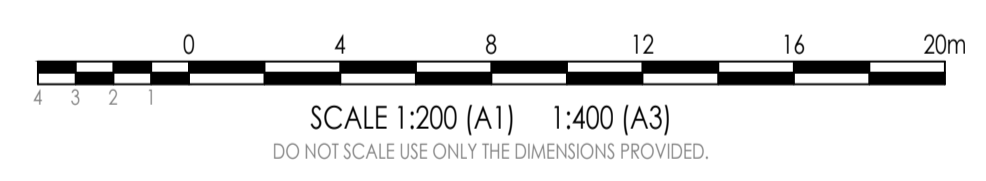
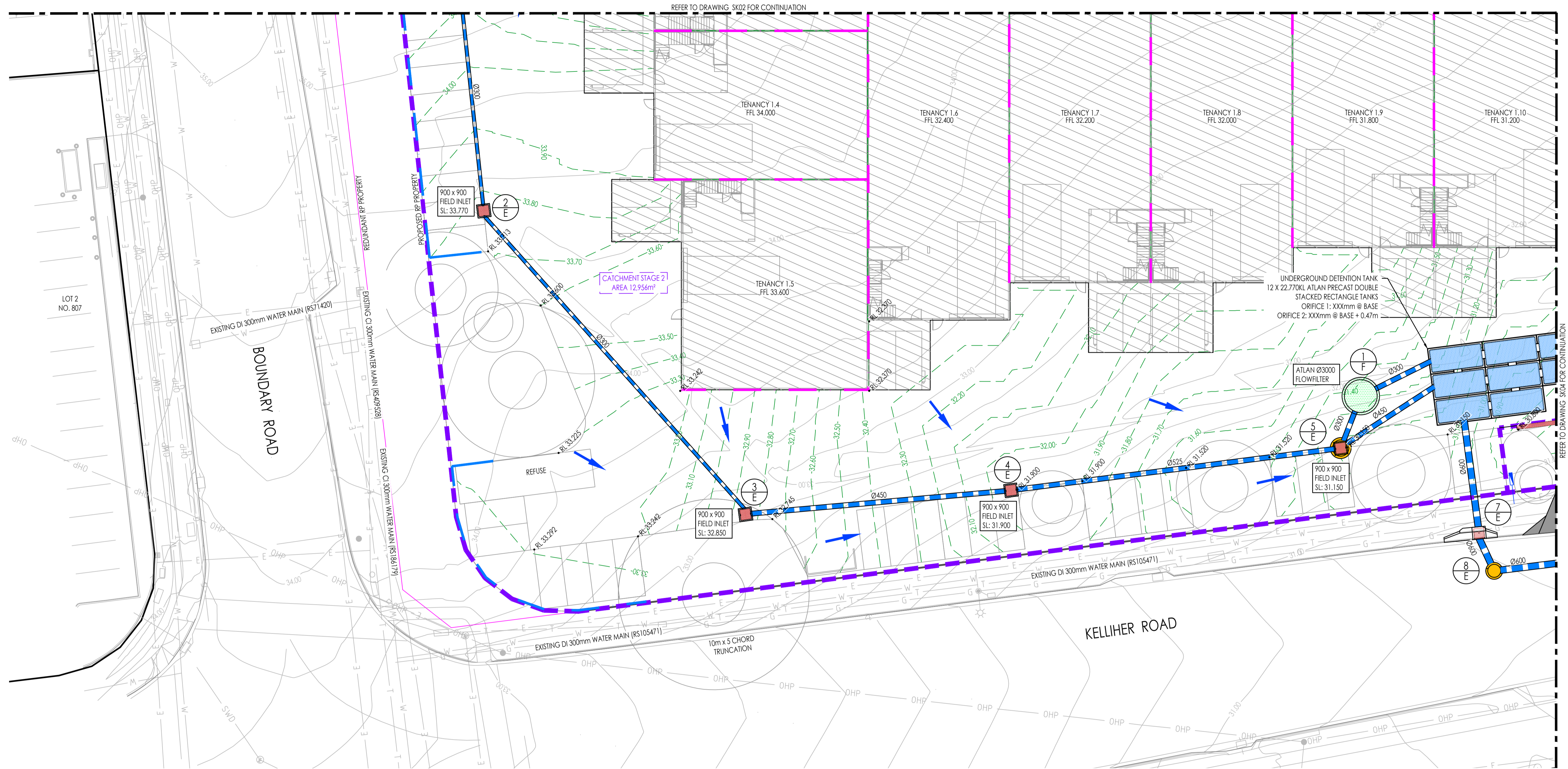
PROJECT:  
804 - 816 BOUNDARY ROAD, RICHLANDS QLD 4077

TITLE:  
CONCEPT STORMWATER DRAINAGE LAYOUT PLAN SHEET 2 OF 4

**APPROVAL**

JOB NO.	C26011
DRAWING NO.	SK03
REVISION:	A





CONCEPT STORMWATER DRAINAGE LAYOUT PLAN SHEET 4 OF 4  
SCALE 1:200

LEGEND	
	EXISTING SURFACE CONTOURS
	FINISHED SURFACE CONTOURS
	PROPOSED LOT BOUNDARY
	REDUNDANT LOT BOUNDARY
	EXISTING LOT BOUNDARY
	EXISTING STORMWATER DRAINAGE
	EXISTING SEWER RETICULATION
	EXISTING WATER RETICULATION
	EXISTING UNDERGROUND POWER
	EXISTING OVERHEAD POWER
	EXISTING TELSTRA LINE
	EXISTING NBN LINE
	EXISTING GAS LINE
	PROPOSED STORMWATER DRAINAGE
	STORMWATER STRUCTURE NUMBER
	STORMWATER CATCHMENT AREA
	STAGING BOUNDARY
	STORMWATER FIELD INLET
	STORMWATER MANHOLE
	STORMWATER CATCHPITS
	STORMWATER FLOW ARROWS
	RETAINING WALL
	BUILDING RETAINING WALL
	DRIVEWAY CROSSOVER HATCH
	EASEMENT

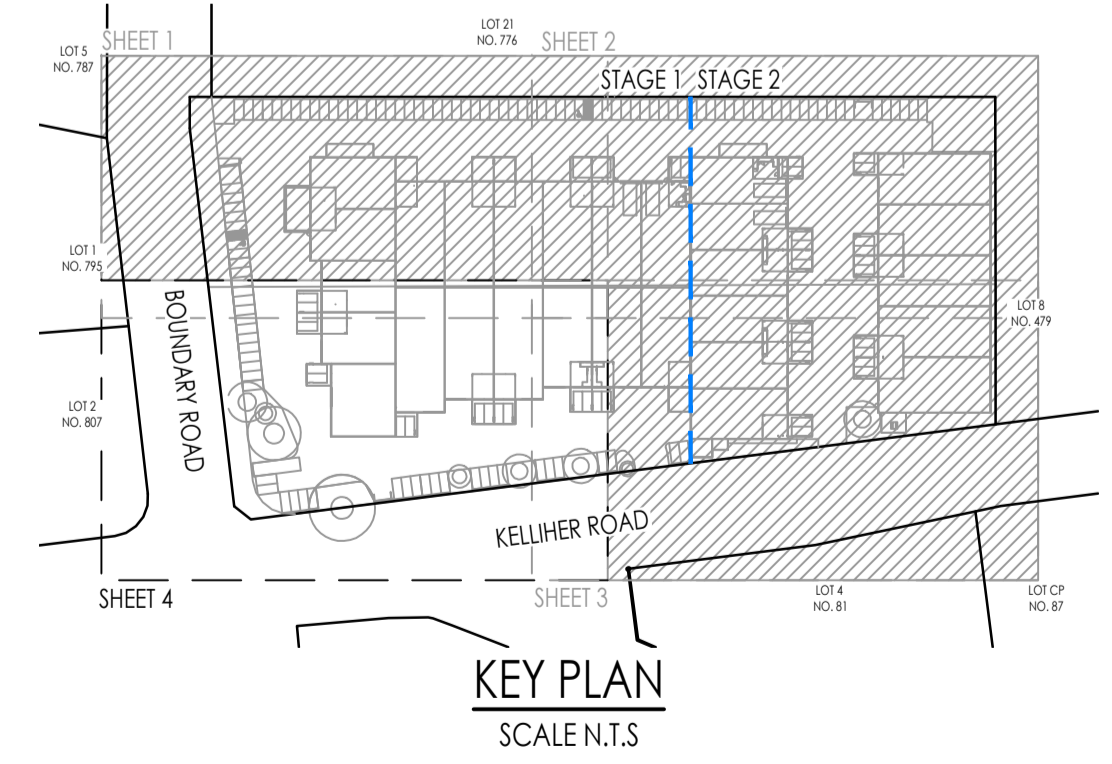
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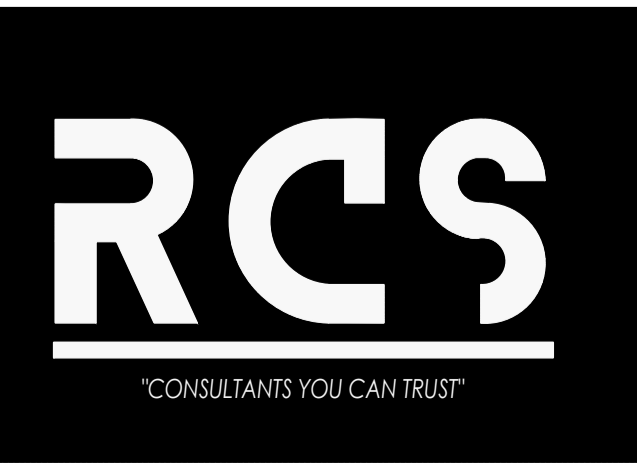
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AMENDMENTS				
NO.	DATE	DESCRIPTION	DRAWN	
A	11.05.2026	PRELIMINARY ISSUE	KN	

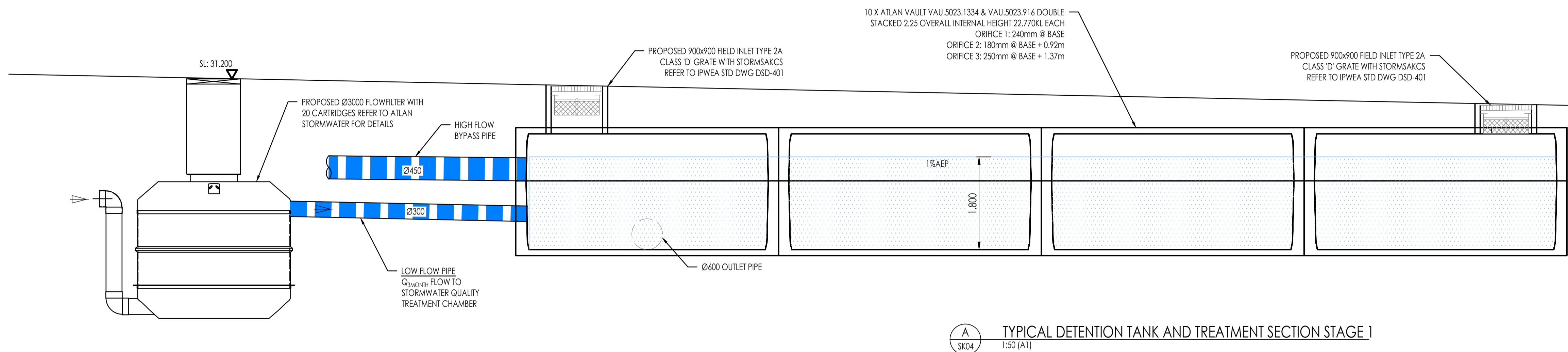


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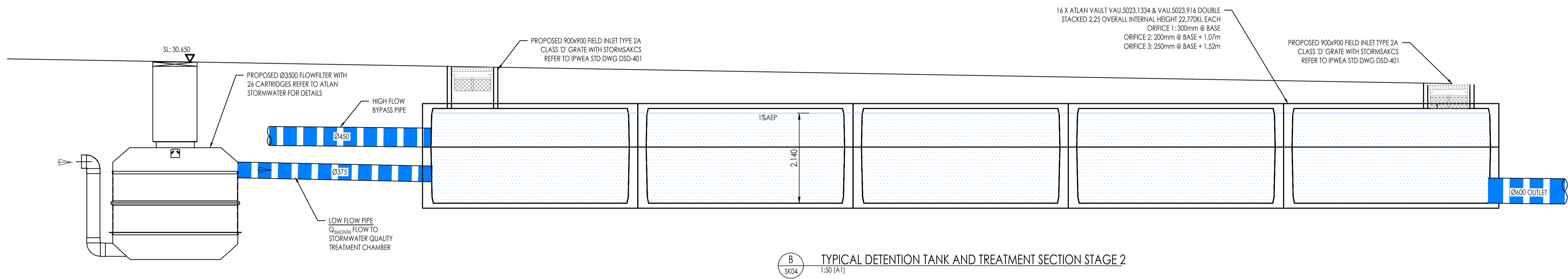
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NORTH: 	RP DESCRIPTION: LOT 1 & 2 ON RP83710	CLIENT: ALSAPCE PROPERTY GROUP UNIT TRUST
APPROVED: On behalf of Rodgers Consulting Services RPEQ. DATE:	SCALE: 1 : 200	ORIG. DRG. SIZE A1
CHECKED: TS	DRAWN: KN	DESIGN: AR
	CHECKED: TS	SUPERVISOR: AR

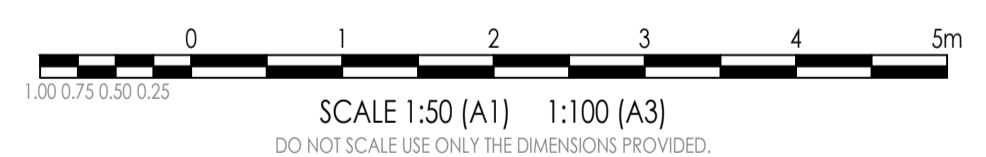
PROJECT: 804 - 816 BOUNDARY ROAD, RICHLANDS QLD 4077	APPROVAL JOB NO. C26011
TITLE: CONCEPT STORMWATER DRAINAGE LAYOUT PLAN SHEET 4 OF 4	DRAWING NO. SK05
	REVISION: A



**A** TYPICAL DETENTION TANK AND TREATMENT SECTION STAGE 1  
SK04 1:50 (A1)



**B** TYPICAL DETENTION TANK AND TREATMENT SECTION STAGE 2  
SK04 1:50 (A1)



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AMENDMENTS				
NO.	DATE	DESCRIPTION	DRAWN	
A	11.05.2026	PRELIMINARY ISSUE	ML	



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**NORTH:**

**RP DESCRIPTION:**  
LOT 1 & 2 ON RP83710

**CLIENT:**  
ALSAPCE PROPERTY GROUP UNIT TRUST

**PROJECT:**  
804 - 816 BOUNDARY ROAD, RICHLANDS QLD 4077

**SCALE:** 1 : 50

**ORIG. DRG. SIZE:** A1

**APPROVED:**  
On behalf of Rodgers Consulting Services

**DATE:**

**DRAWN:** KN

**DESIGN:** AR

**CHECKED:** TS

**SUPERVISOR:** AR

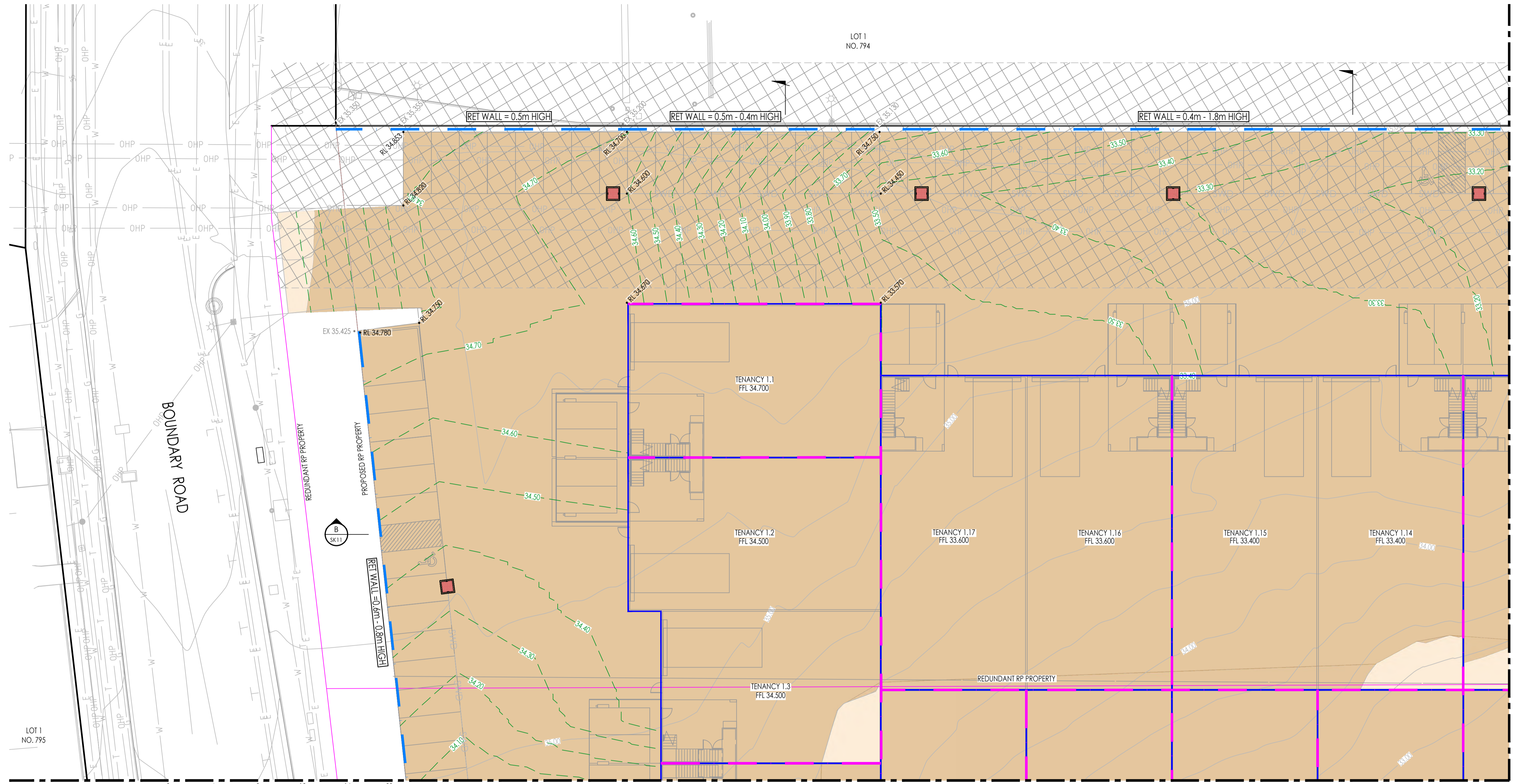
**APPROVAL**

**JOB NO.:** C26011

**DRAWING NO.:** SK06

**REVISION:** A

**TITLE:**  
CONCEPT STORMWATER DRAINAGE DETAILS PLAN

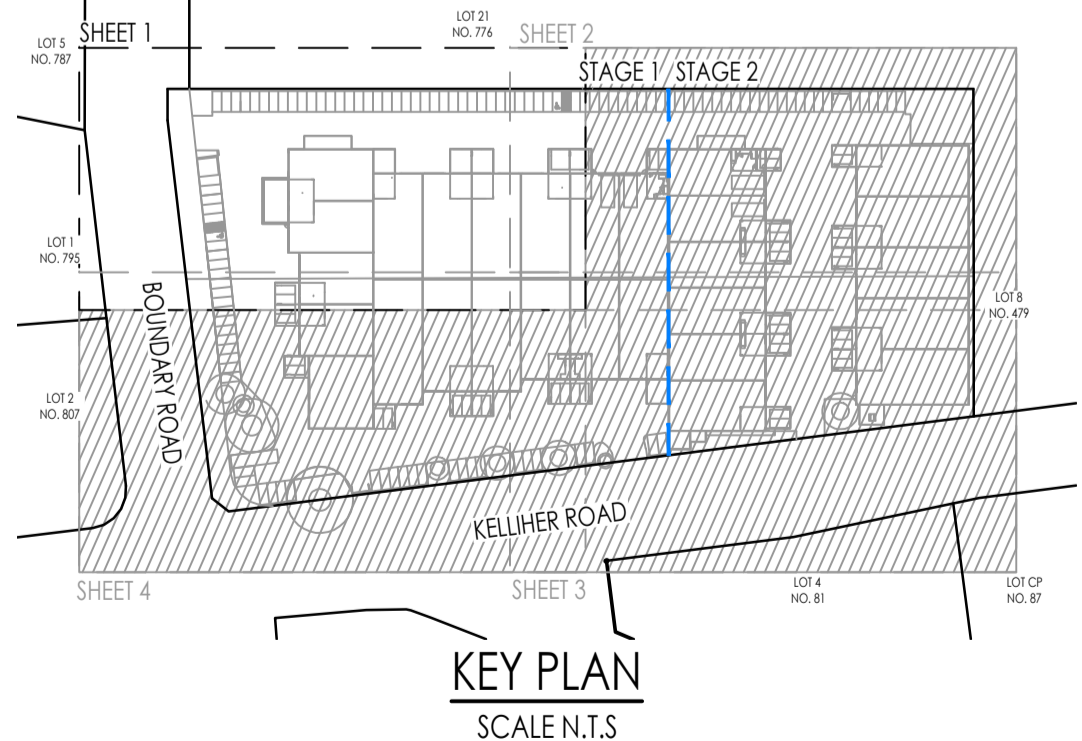


CONCEPT BULK EARTHWORKS LAYOUT PLAN SHEET 1 OF 4  
SCALE 1:200



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**EARTHWORKS TABLE**

SITE PREPARATION	CUT (m³)	FILL (m³)	NET (m³)
SITE STRIPPING (50mm CLEARING + 50mm STRIP)	- 1,868.299	0.000	- 1,868.299 m³
SITE EXCAVATION			
BULK EARTHWORKS (FROM STRIPPED)	- 22,835.114	435.930	-22,399.184 m³
<b>GRAND TOTAL</b>	<b>- 22,835.114 m³</b>	<b>435.930 m³</b>	<b>-22,399.184 m³</b>

NOTE: EARTHWORKS VOLUMES ARE TAKEN FROM EXISTING SURFACE STRIPPED 100mm. CONTRACTOR IS TO CONFIRM VOLUMES PRIOR TO TENDER OR COMMENCEMENT OF WORKS ON SITE.

**LEGEND**

- 5.50--- EXISTING SURFACE CONTOURS
- 5.50--- FINISHED SURFACE CONTOURS
- PROPOSED LOT BOUNDARY
- REDUNDANT LOT BOUNDARY
- EXISTING LOT BOUNDARY
- SWD EXISTING STORMWATER DRAINAGE
- S EXISTING SEWER RETICULATION
- W EXISTING WATER RETICULATION
- E EXISTING UNDERGROUND POWER
- OHP EXISTING OVERHEAD POWER
- T EXISTING TELSTRA LINE
- EXISTING NBN LINE
- EXISTING GAS LINE
- RETAINING WALL
- BUILDING RETAINING WALL
- EXISTING NBN LINE
- EXISTING GAS LINE
- PROPOSED STORMWATER DRAINAGE
- DENOTES AREA OF CUT
- DENOTES AREA OF FILL
- DENOTES EASEMENT AREA
- EX EXISTING SURFACE LEVEL
- RL PROPOSED DESIGN LEVEL

AMENDMENTS				
NO.	DATE	DESCRIPTION	DRAWN	
A	11.05.2026	PRELIMINARY ISSUE	KN	



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NORTH:

RP DESCRIPTION: LOT 1 & 2 ON RP83710

CLIENT: ALSPACE PROPERTY GROUP UNIT TRUST

SCALE: 1 : 200

ORIG. DRG. SIZE: A1

APPROVED: On behalf of Rodgers Consulting Services

CHECKED: TS

DATE:

DRAWN: KN

DESIGN: AR

SUPERVISOR: AR

PROJECT: 804 - 816 BOUNDARY ROAD, RICHLANDS QLD 4077

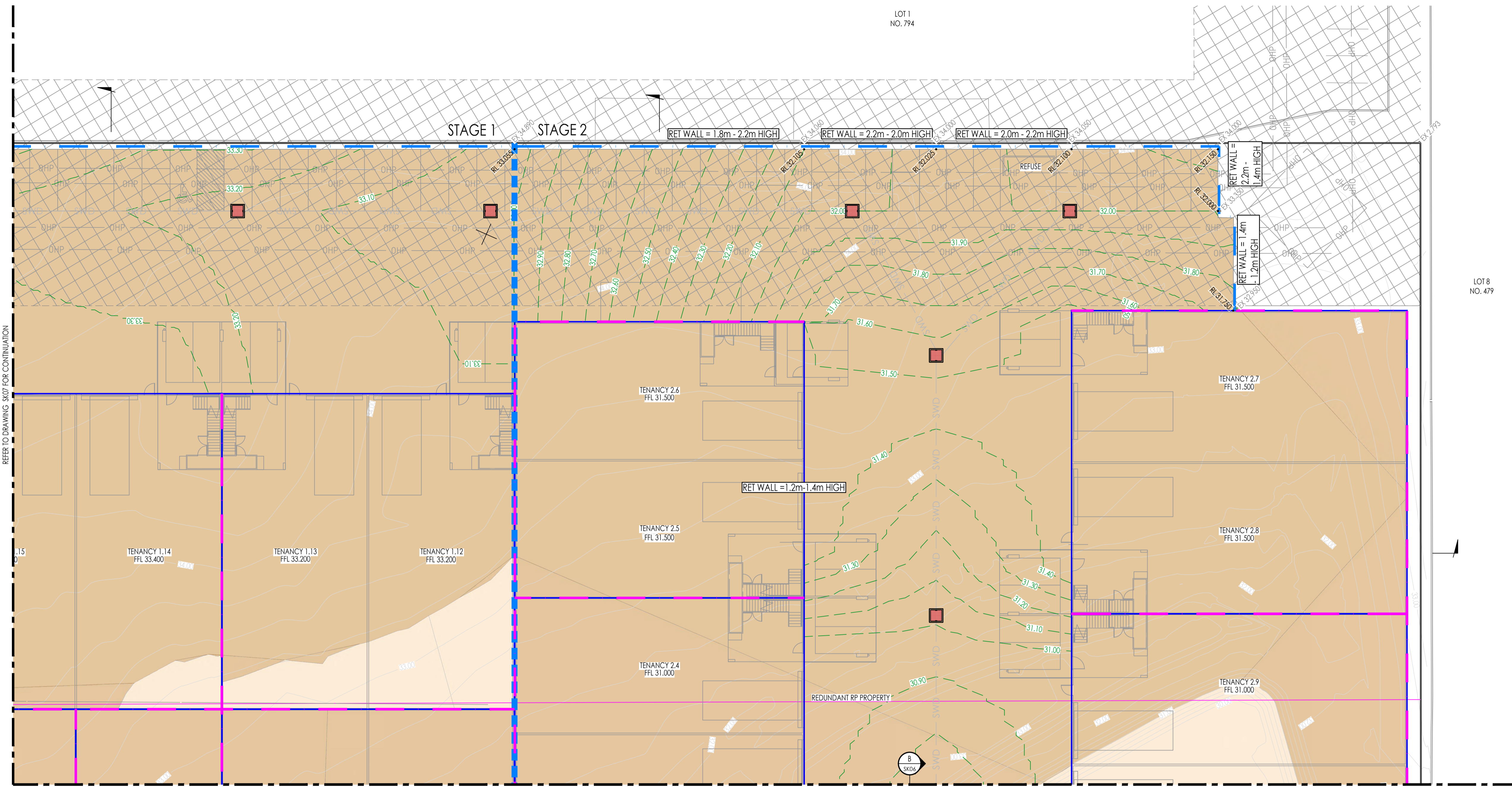
TITLE: CONCEPT BULK EARTHWORKS LAYOUT PLAN SHEET 1 OF 4

**APPROVAL**

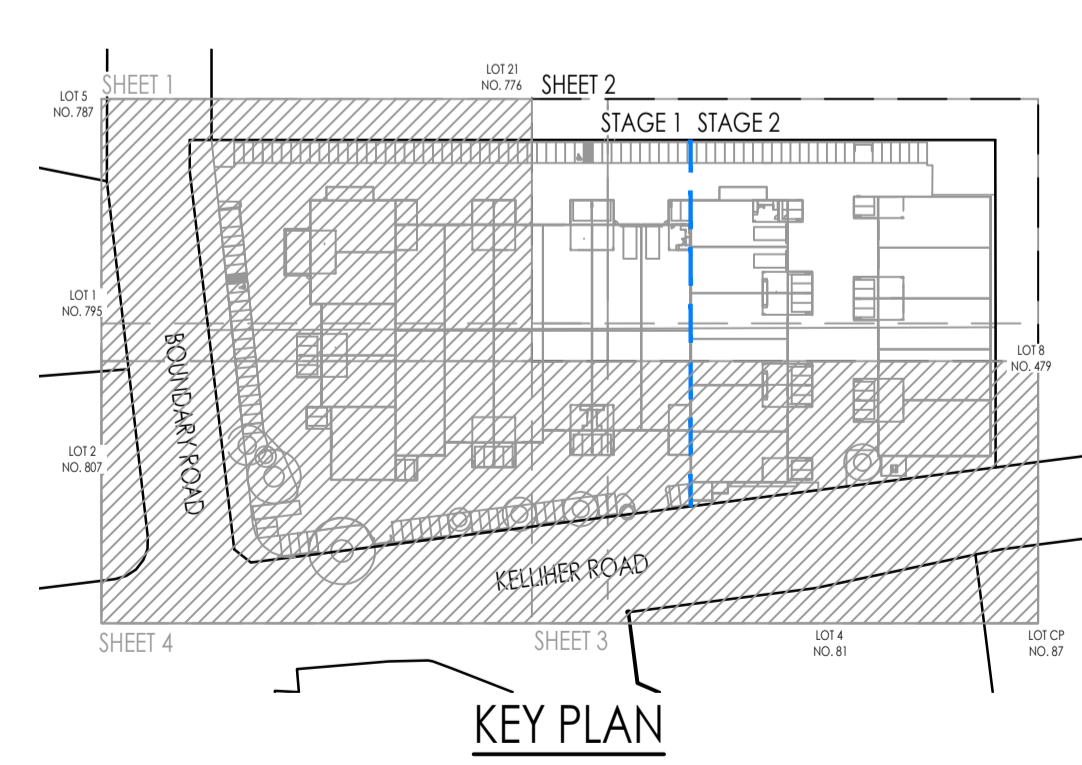
JOB NO. C26011

DRAWING NO. SK07

REVISION: A



CONCEPT BULK EARTHWORKS LAYOUT PLAN SHEET 2 OF 4  
SCALE 1:200



LEGEND	
---5.50---	EXISTING SURFACE CONTOURS
---5.50---	FINISHED SURFACE CONTOURS
---	PROPOSED LOT BOUNDARY
---	REDUNDANT LOT BOUNDARY
---	EXISTING LOT BOUNDARY
SWD	EXISTING STORMWATER DRAINAGE
S	EXISTING SEWER RETICULATION
W	EXISTING WATER RETICULATION
E	EXISTING UNDERGROUND POWER
OHP	EXISTING OVERHEAD POWER
T	EXISTING TELSTRA LINE
---	EXISTING NBN LINE
G	EXISTING GAS LINE
---	RETAINING WALL
---	BUILDING RETAINING WALL
---	STAGING BOUNDARY
EX 40.022	EX = EXISTING SURFACE LEVEL
RL 41.399	RL = PROPOSED DESIGN LEVEL
---	PROPOSED STORMWATER DRAINAGE
---	DENOTES AREA OF CUT
---	DENOTES AREA OF FILL
---	DENOTES EASEMENT AREA

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AMENDMENTS				
NO.	DATE	DESCRIPTION	DRAWN	
A	11.05.2026	PRELIMINARY ISSUE	KN	



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NORTH:

RP DESCRIPTION:  
LOT 1 & 2 ON RP83710

CLIENT:  
ALSAPCE PROPERTY GROUP UNIT TRUST

SCALE: 1 : 200

ORIG. DRG. SIZE  
A1

APPROVED:  
On behalf of Rodgers Consulting Services  
RPEQ. DATE:

DRAWN:  
KN

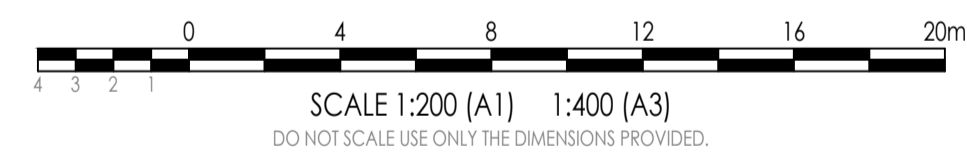
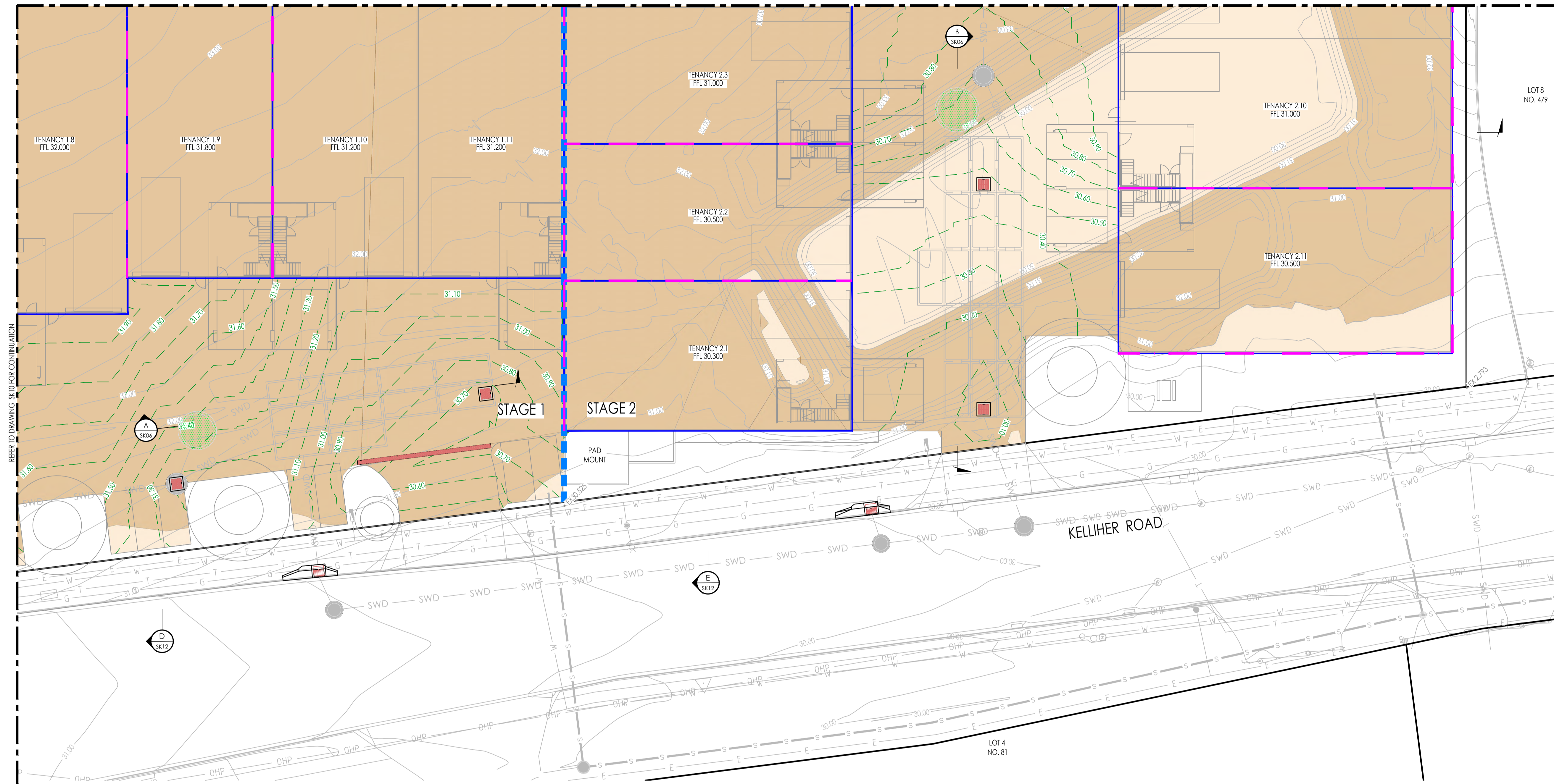
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CHECKED:  
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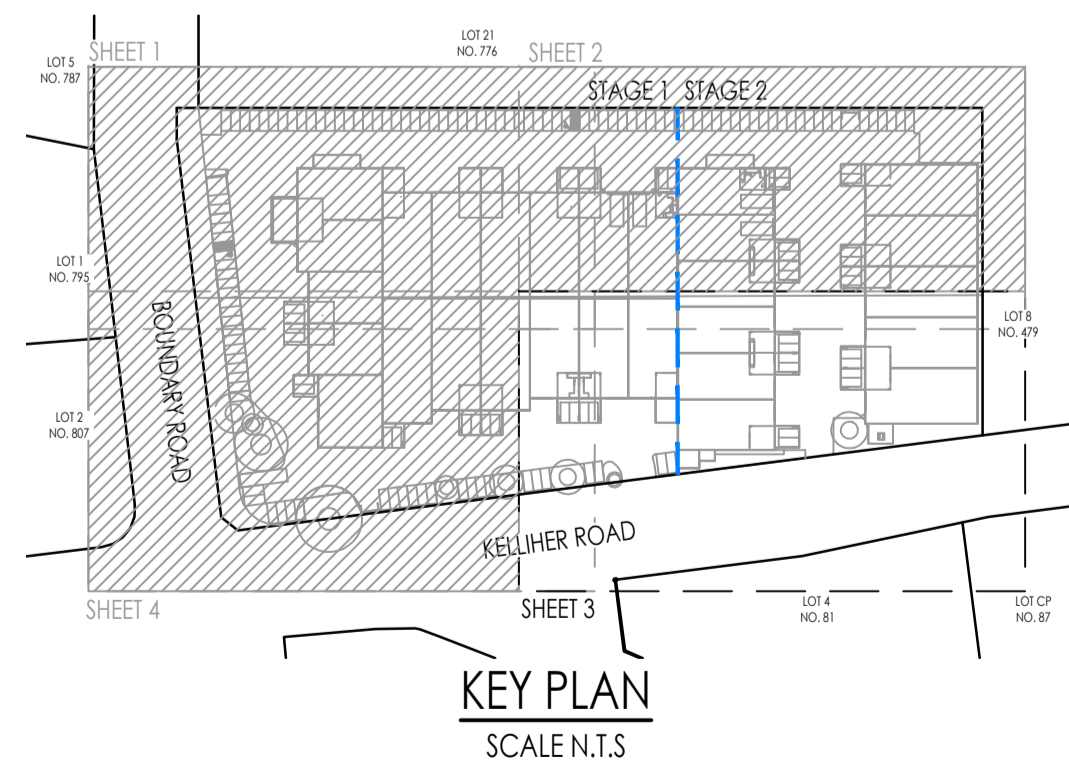
SUPERVISOR:  
AR

PROJECT: 804 - 816 BOUNDARY ROAD, RICHLANDS QLD 4077	APPROVAL JOB NO. C26011 DRAWING NO. SK08 REVISION: A
TITLE: CONCEPT BULK EARTHWORKS LAYOUT PLAN SHEET 2 OF 4	

REFER TO DRAWING SK08 FOR CONTINUATION



CONCEPT BULK EARTHWORKS LAYOUT PLAN SHEET 3 OF 4  
SCALE 1:200



LEGEND

- - - -5.50- - - - EXISTING SURFACE CONTOURS
- - - -5.50- - - - FINISHED SURFACE CONTOURS
- — — — PROPOSED LOT BOUNDARY
- — — — REDUNDANT LOT BOUNDARY
- — — — EXISTING LOT BOUNDARY
- SWD — — — — EXISTING STORMWATER DRAINAGE
- S — — — — EXISTING SEWER RETICULATION
- W — — — — EXISTING WATER RETICULATION
- E — — — — EXISTING UNDERGROUND POWER
- OHP — — — — EXISTING OVERHEAD POWER
- T — — — — EXISTING TELSTRA LINE
- HBN — — — — EXISTING HBN LINE
- G — — — — EXISTING GAS LINE
- — — — — RETAINING WALL
- — — — — BUILDING RETAINING WALL
- — — — — STAGING BOUNDARY
- — — — — EX - EXISTING SURFACE LEVEL
- — — — — RL - PROPOSED DESIGN LEVEL
- SWD — — — — PROPOSED STORMWATER DRAINAGE
- — — — — DENOTES AREA OF CUT
- — — — — DENOTES AREA OF FILL
- — — — — DENOTES EASEMENT AREA

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NORTH:

RP DESCRIPTION:  
LOT 1 & 2 ON RP83710

CLIENT:  
ALSAPCE PROPERTY GROUP UNIT TRUST

PROJECT:  
804 - 816 BOUNDARY ROAD, RICHLANDS QLD 4077

APPROVED:  
On behalf of Rodgers Consulting Services

DATE:

SCALE: 1 : 200

ORIG. DRG. SIZE  
A1

DRAWN:  
KN

DESIGN:  
AR

CHECKED:  
TS

SUPERVISOR:  
AR

PROJECT:  
804 - 816 BOUNDARY ROAD, RICHLANDS QLD 4077

CLIENT:  
ALSAPCE PROPERTY GROUP UNIT TRUST

APPROVAL

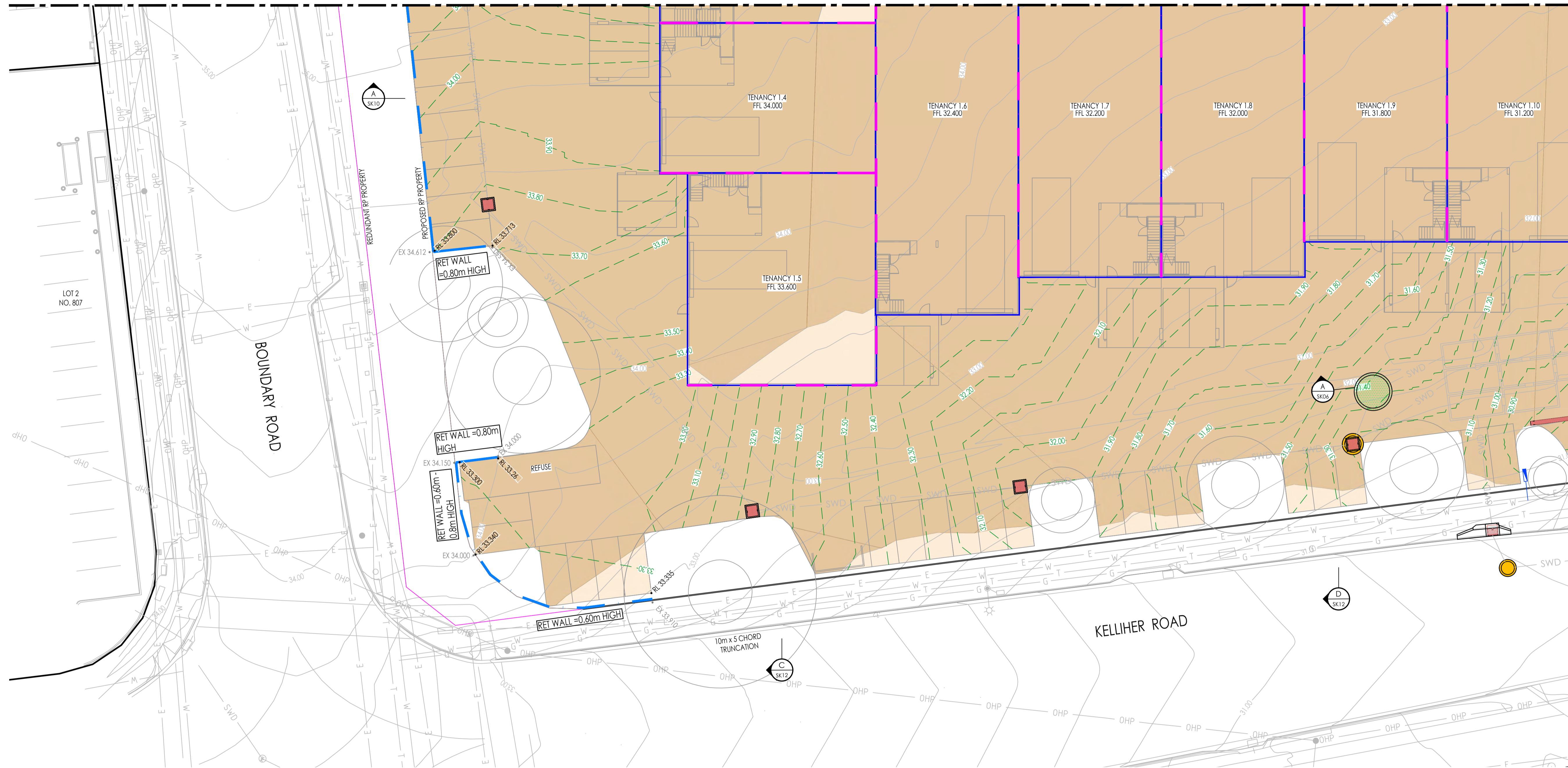
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DRAWING NO.  
SK09

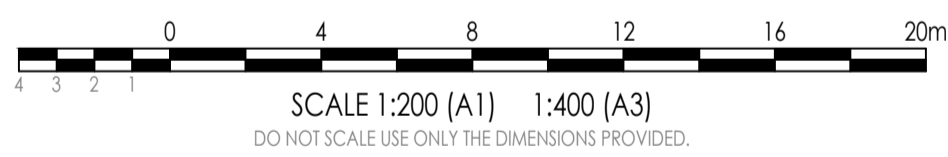
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REFER TO DRAWING SK07 FOR CONTINUATION



REFER TO DRAWING SK07 FOR CONTINUATION

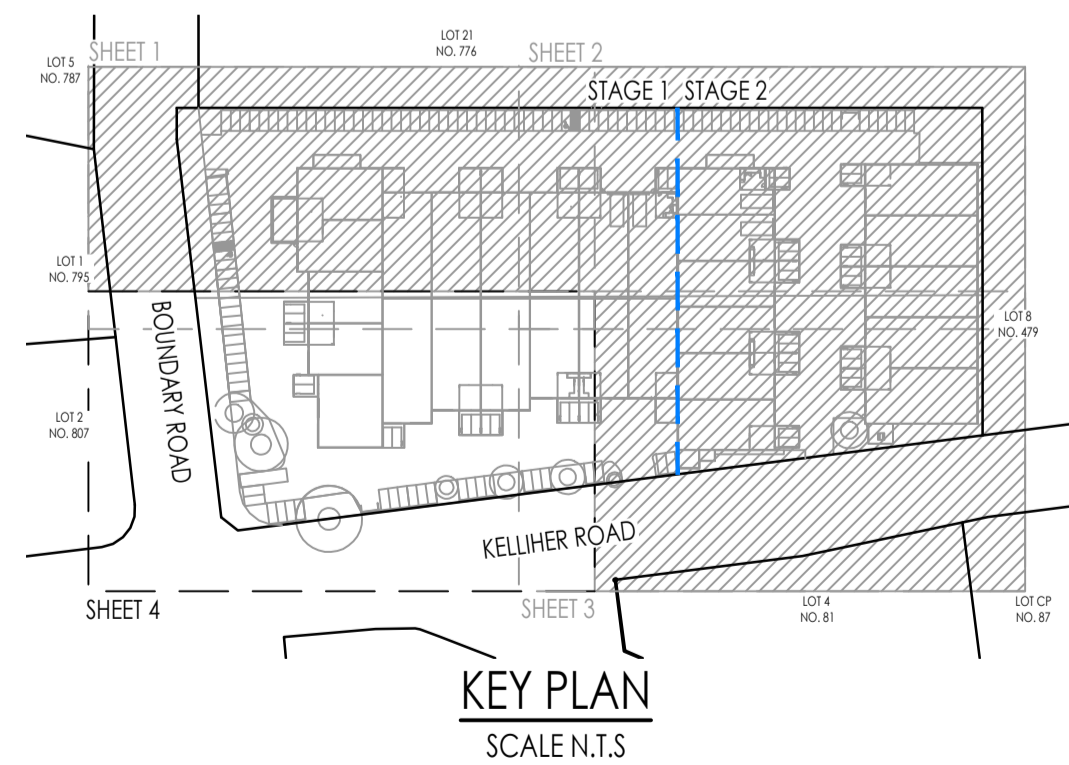


CONCEPT BULK EARTHWORKS LAYOUT PLAN SHEET 4 OF 4  
SCALE 1:200

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LEGEND

- - - -5.50- - - - EXISTING SURFACE CONTOURS
- - - -5.50- - - - FINISHED SURFACE CONTOURS
- PROPOSED LOT BOUNDARY
- REDUNDANT LOT BOUNDARY
- EXISTING LOT BOUNDARY
- SWD — EXISTING STORMWATER DRAINAGE
- S — EXISTING SEWER RETICULATION
- W — EXISTING WATER RETICULATION
- E — EXISTING UNDERGROUND POWER
- OHP — EXISTING OVERHEAD POWER
- T — EXISTING TELSTRA LINE
- NBN — EXISTING NBN LINE
- G — EXISTING GAS LINE
- RETAINING WALL
- BUILDING RETAINING WALL
- EX — EXISTING SURFACE LEVEL
- RL — PROPOSED DESIGN LEVEL
- SWD — PROPOSED STORMWATER DRAINAGE
- DENOTES AREA OF CUT
- DENOTES AREA OF FILL
- DENOTES EASEMENT AREA

AMENDMENTS				
NO.	DATE	DESCRIPTION	DRAWN	
A	11.05.2026	PRELIMINARY ISSUE	KN	



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NORTH:

RP DESCRIPTION:  
LOT 1 & 2 ON RP83710

CLIENT:  
ALSAPCE PROPERTY GROUP UNIT TRUST

SCALE: 1 : 200

ORIG. DRG. SIZE  
A1

APPROVED:  
On behalf of Rodgers Consulting Services

DRAWN: KN  
DESIGN: AR

CHECKED: TS  
SUPERVISOR: AR

RPEQ. DATE:

PROJECT:  
804 - 816 BOUNDARY ROAD, RICHLANDS QLD 4077

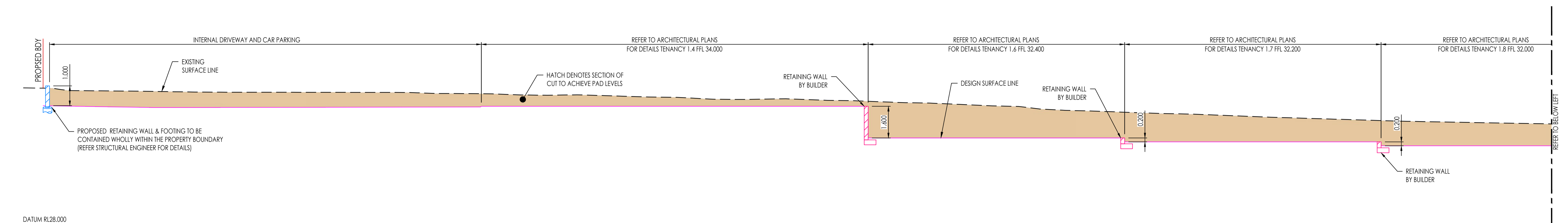
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CONCEPT BULK EARTHWORKS LAYOUT PLAN SHEET 4 OF 4

APPROVAL

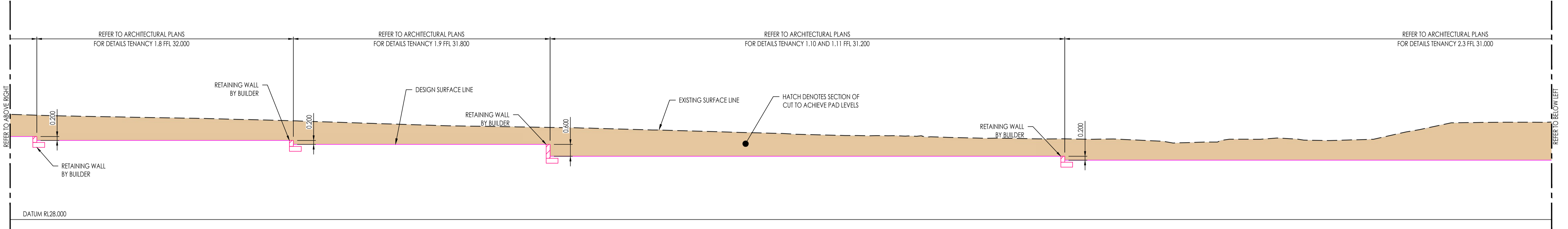
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C26011

DRAWING NO.  
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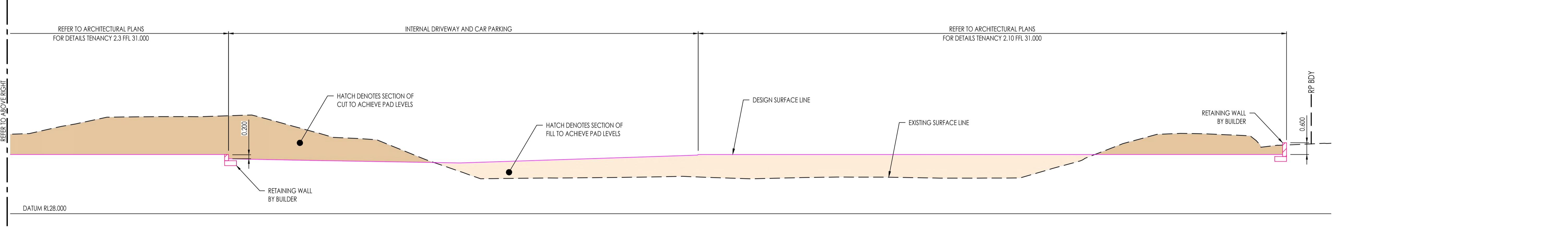
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SECTION A SK10  
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SECTION A SK10  
1:100 (A1)



SECTION A SK10  
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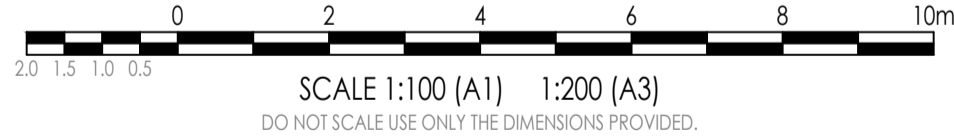
PROPOSED AREAS OF FILL

PROPOSED AREAS OF CUT

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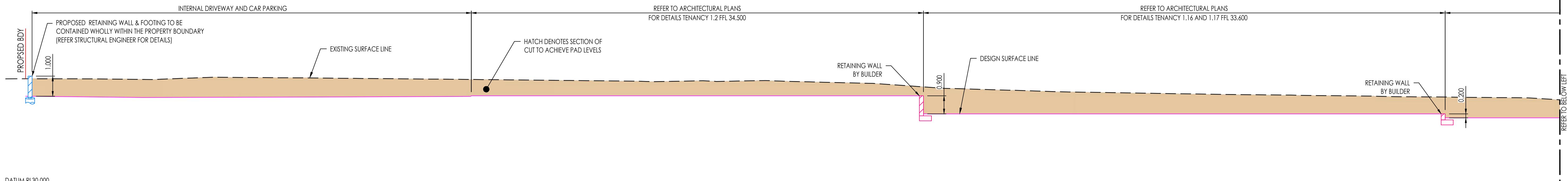


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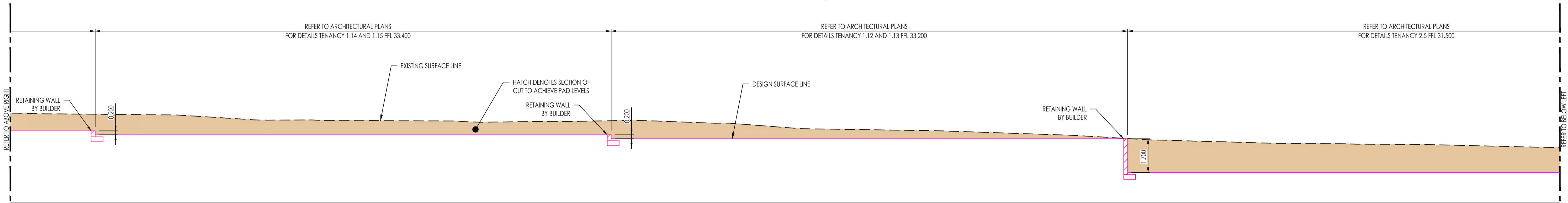
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**CONCEPT BULK EARTHWORKS SECTION PLAN SHEET 1 OF 4**  
SCALE 1:100

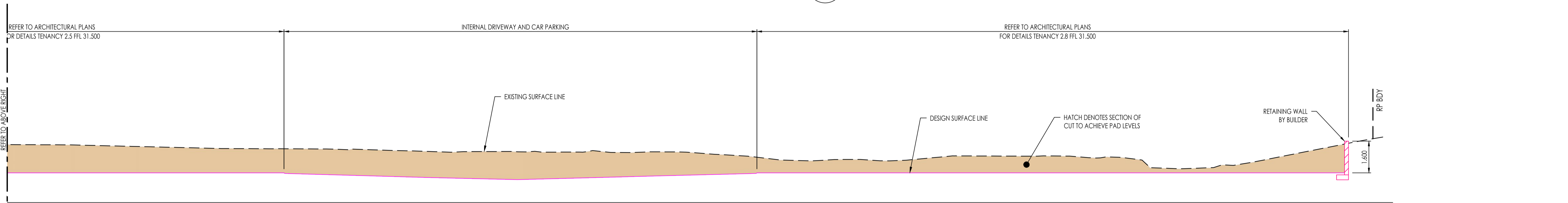
AMENDMENTS						<small>DISCLAIMER NOTE:</small> THIS PLAN WAS PREPARED FOR DISCUSSION AND ESTIMATING PURPOSES. THE LAYOUT SHOWN IS INDICATIVE ONLY AND MAY BE SUBJECT TO LOCAL AUTHORITY / GOVERNMENT REQUIREMENTS AND FURTHER DETAILED ENGINEERING DESIGN. THIS PLAN SHALL NOT BE USED FOR CONSTRUCTION PURPOSES AND IS NOT TO BE USED FOR ANY OTHER PURPOSE OR BY ANY OTHER PERSON OR CORPORATION OTHER THAN LISTED AS THE 'CLIENT' ON THIS PLAN. RODGERS CONSULTING SERVICES ACCEPTS NO RESPONSIBILITY FOR ANY LOSS OR DAMAGE SUFFERED HOWEVER SO ARISING TO ANY PERSON OR CORPORATION WHO MAY USE OR RELY ON THIS PLAN IN CONTRAVENTION OF THE TERMS OF THIS CLAUSE. ALL COPIES OF THIS PLAN MUST INCLUDE THIS DISCLAIMER.  <small>IMAGERY, CADASTRAL AND SURVEY DATA DISCLAIMER</small> WHILE EVERY CARE IS TAKEN BY RODGERS CONSULTING SERVICES TO ENSURE THE ACCURACY OF THE DATA SUPPLIED BY LOCAL AUTHORITIES, BEFORE YOU DIG (BYD) RECORDS, AS CONSTRUCTED INFORMATION AND DETAILED SURVEY, WE MAKE NO REPRESENTATIONS OR WARRANTIES ABOUT ITS ACCURACY, RELIABILITY, COMPLETENESS OR SUITABILITY FOR ANY PARTICULAR PURPOSE AND DISCLAIM ALL RESPONSIBILITY AND ALL LIABILITY (INCLUDING WITHOUT LIMITATION, LIABILITY IN NEGLIGENCE) FOR ALL EXPENSES, LOSSES, DAMAGES (INCLUDING INDIRECT OR CONSEQUENTIAL DAMAGE) AND COSTS WHICH MAY BE INCURRED AS A RESULT OF DATA BEING INACCURATE OR INCOMPLETE IN ANY WAY AND FOR ANY REASON.	<small>NORTH:</small> <small>RP DESCRIPTION:</small> LOT 1 & 2 ON RP83710 <small>CLIENT:</small> ALSAPCE PROPERTY GROUP UNIT TRUST <small>PROJECT:</small> 804 - 816 BOUNDARY ROAD, RICHLANDS QLD 4077 <small>TITLE:</small> CONCEPT BULK EARTHWORKS SECTION PLAN SHEET 1 OF 4	APPROVAL	
NO.	DATE	DESCRIPTION	DRAWN	JOB NO.					
A	11.05.2026	PRELIMINARY ISSUE	KN	C26011					
				DRAWING NO. SK11					
				REVISION: A					



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SECTION B  
1:100 (A1) SK07



SECTION B  
1:100 (A1) SK07

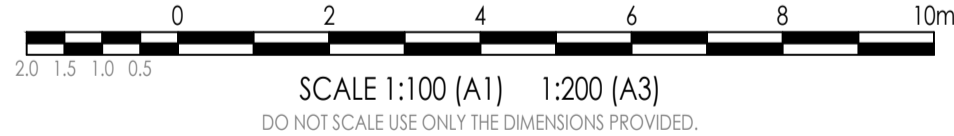
**LEGEND**

- PROPOSED AREAS OF FILL
- PROPOSED AREAS OF CUT

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**CONCEPT BULK EARTHWORKS SECTION PLAN SHEET 2 OF 4**  
SCALE 1:100

AMENDMENTS				
NO.	DATE	DESCRIPTION	DRAWN	
A	11.05.2026	PRELIMINARY ISSUE	KN	



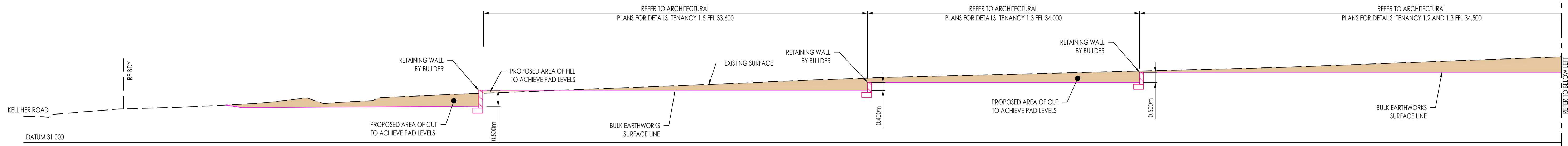
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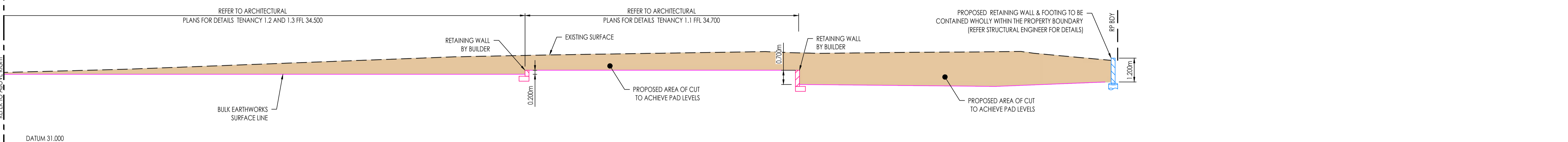
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APPROVED: On behalf of Rodgers Consulting Services	SCALE: 1 : 100	ORIG. DRG. SIZE A1
RPEQ. DATE:	DRAWN: KN	DESIGN: AR
	CHECKED: TS	SUPERVISOR: AR

PROJECT: 804 - 816 BOUNDARY ROAD, RICHLANDS QLD 4077
TITLE: CONCEPT BULK EARTHWORKS SECTION PLAN SHEET 2 OF 4

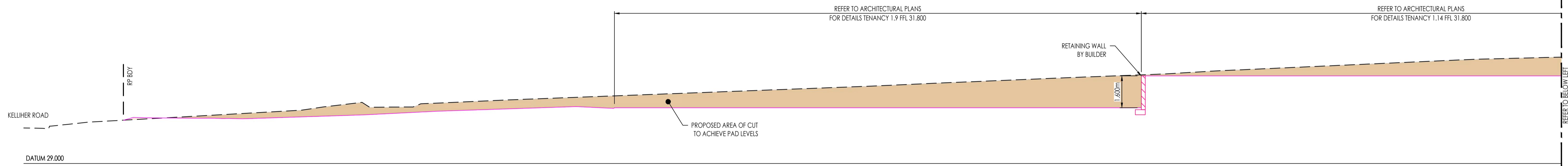
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JOB NO. C26011	DRAWING NO. SK12
REVISION: A	



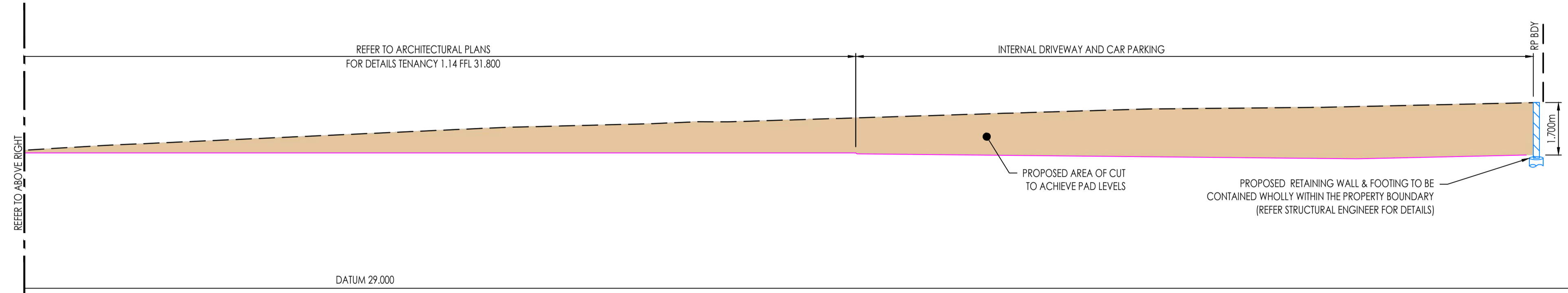
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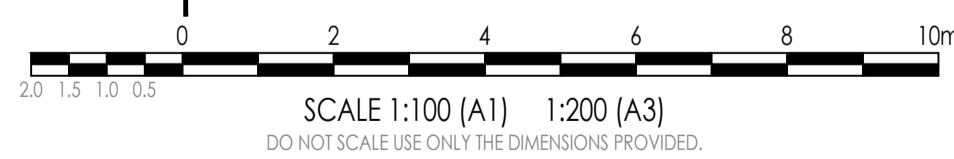
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SECTION D  
1:100 (A1) SK10



SECTION D  
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CONCEPT BULK EARTHWORKS SECTION PLAN SHEET 3 OF 4

**LEGEND**

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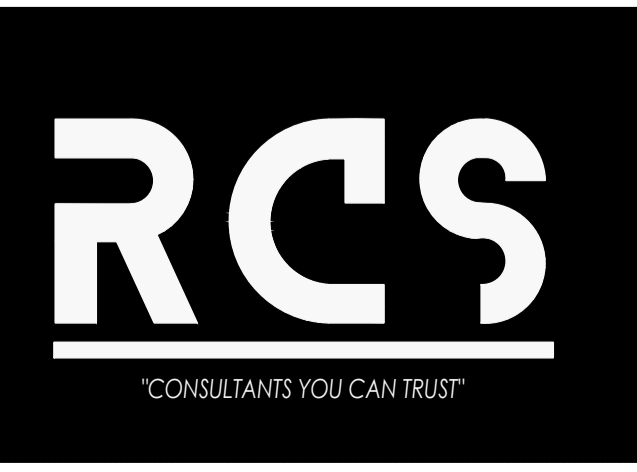
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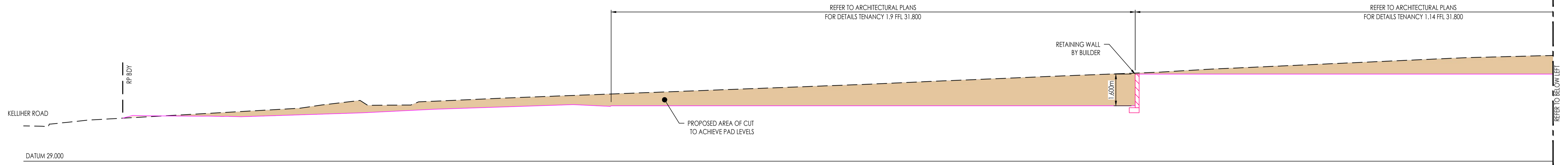
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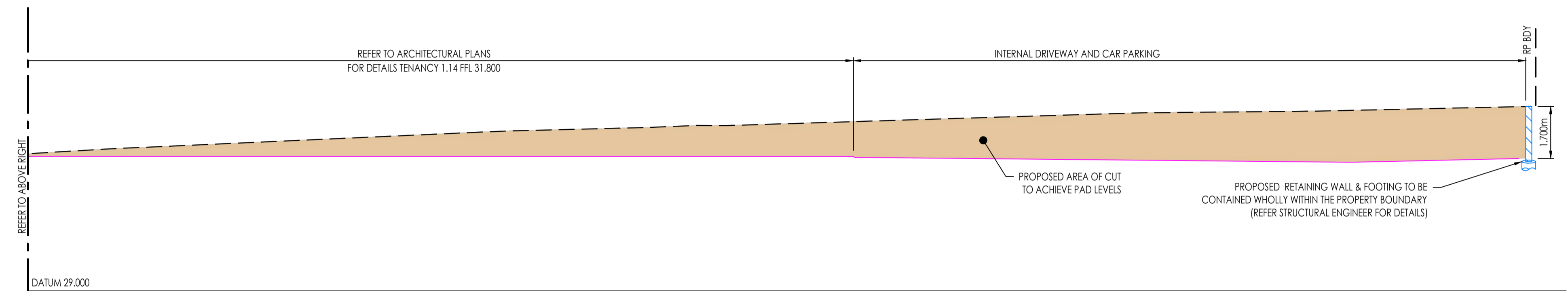
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APPROVED: On behalf of Rodgers Consulting Services	SCALE: 1 : 100	ORIG. DRG. SIZE A1
DATE:	DRAWN: KN	DESIGN: AR
	CHECKED: TS	SUPERVISOR: AR

PROJECT: 804 - 816 BOUNDARY ROAD, RICHLANDS QLD 4077
TITLE: CONCEPT BULK EARTHWORKS SECTION PLAN SHEET 3 OF 4

<b>APPROVAL</b>	
JOB NO. C26011	DRAWING NO. SK13
REVISION: A	



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1:100 (A1)

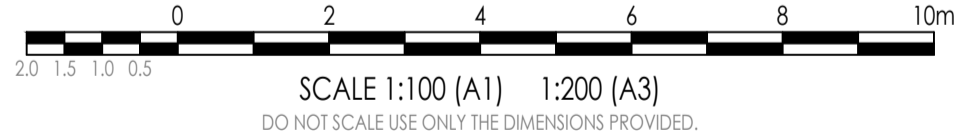


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1:100 (A1)

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CONCEPT BULK EARTHWORKS SECTION PLAN SHEET 4 OF 4  
SCALE 1:100

**LEGEND**

- PROPOSED AREAS OF FILL
- PROPOSED AREAS OF CUT

AMENDMENTS				
NO.	DATE	DESCRIPTION	DRAWN	
A	11.05.2026	PRELIMINARY ISSUE	KN	



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**NORTH:**

**RP DESCRIPTION:**  
LOT 1 & 2 ON RP83710

**CLIENT:**  
ALSSPACE PROPERTY GROUP UNIT TRUST

**PROJECT:**  
804 - 816 BOUNDARY ROAD, RICHLANDS QLD 4077

**SCALE:** 1 : 100

**ORIG. DRG. SIZE:** A1

**APPROVED:**  
On behalf of Rodgers Consulting Services

**DATE:**

**DRAWN:** KN

**DESIGN:** AR

**CHECKED:** TS

**SUPERVISOR:** AR

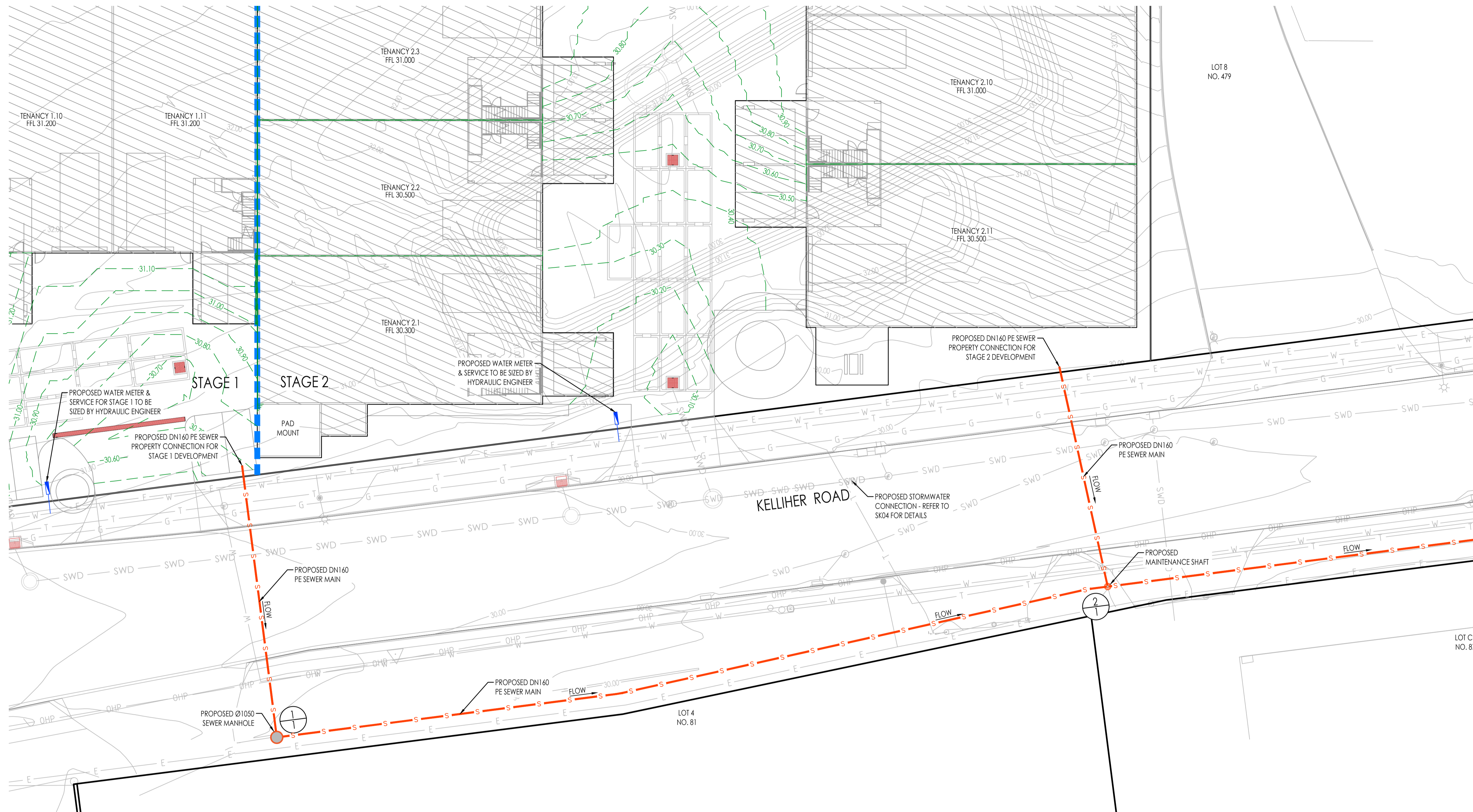
**APPROVAL**

**JOB NO.:** C26011

**DRAWING NO.:** SK14

**REVISION:** A

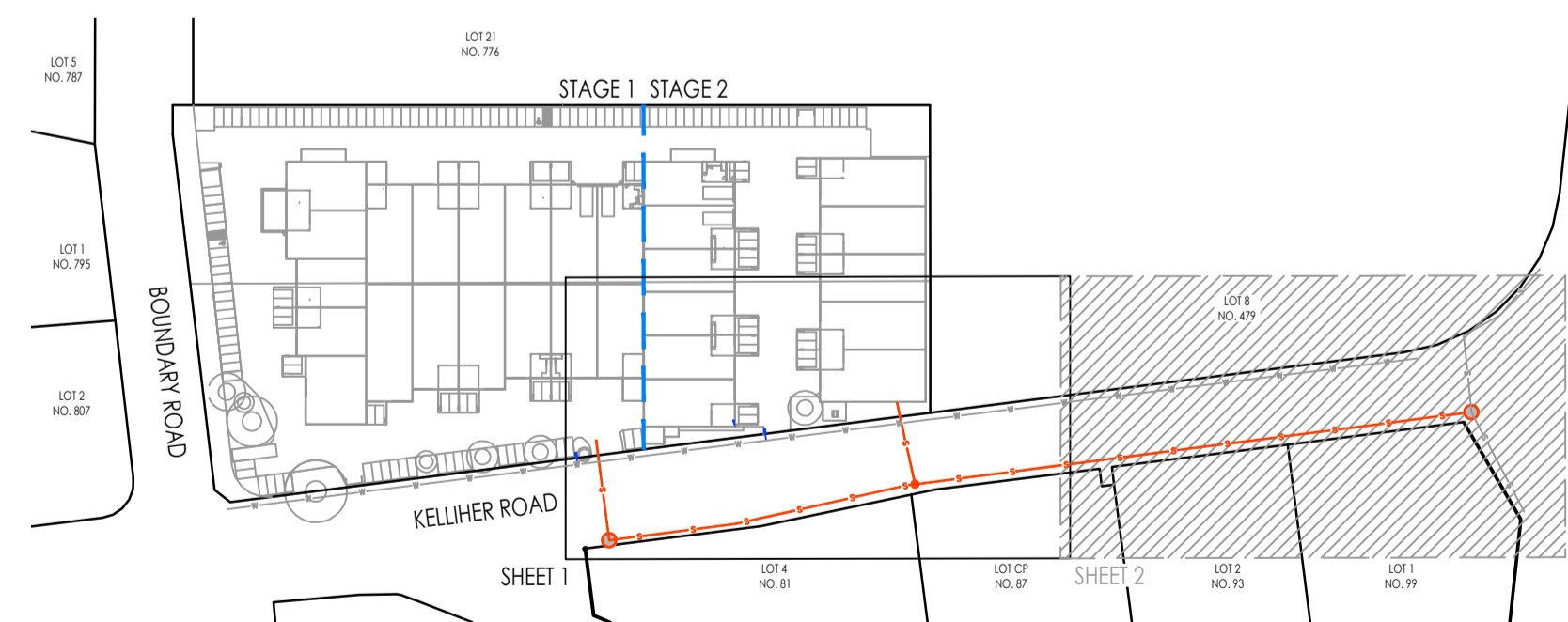
**TITLE:**  
CONCEPT BULK EARTHWORKS SECTION PLAN SHEET 4 OF 4



CONCEPT SEWER & WATER RETICULATION LAYOUT PLAN SHEET 1 OF 2  
SCALE 1:200

LEGEND

- 5.50--- EXISTING SURFACE CONTOURS
- - -5.50- - - FINISHED SURFACE CONTOURS
- PROPOSED LOT BOUNDARY
- EXISTING LOT BOUNDARY
- SWD --- EXISTING STORMWATER DRAINAGE
- S --- EXISTING SEWER RETICULATION
- W --- EXISTING WATER RETICULATION
- E --- EXISTING UNDERGROUND POWER
- OHP --- EXISTING OVERHEAD POWER
- T --- EXISTING TELSTRA LINE
- NBN --- EXISTING NBN LINE
- G --- EXISTING GAS LINE
- STAGING BOUNDARY
- S --- PROPOSED SEWER RETICULATION
- W --- PROPOSED WATER RETICULATION
- MAINTENANCE HOLE/ SHAFT
- B1 UPRIGHT KERB AND CHANNEL
- SEWER STRUCTURE NUMBER
- DRIVEWAY CROSSOVER HATCH
- DRIVEWAY HATCH
- STORMWATER FIELD INLET
- STORMWATER MANHOLE
- EASEMENT



KEY PLAN  
SCALE N.T.S

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LOCATION AND LEVELS OF ALL EXISTING SERVICES AND PROPOSED STORMWATER OUTLETS TO BE CONFIRMED ON SITE BY THE CONTRACTOR PRIOR TO THE COMMENCEMENT OF WORKS. IT IS THE CONTRACTOR'S RESPONSIBILITY TO PERFORM A 'BEFORE YOU DIG' SEARCH AND CARRY OUT ALL INVESTIGATIONS REQUIRED TO SATISFY THEMSELVES OF THE LOCATION OF EXISTING SERVICES. ANY POTENTIAL CONFLICTS SHALL BE REPORTED TO THE SUPERINTENDENT.

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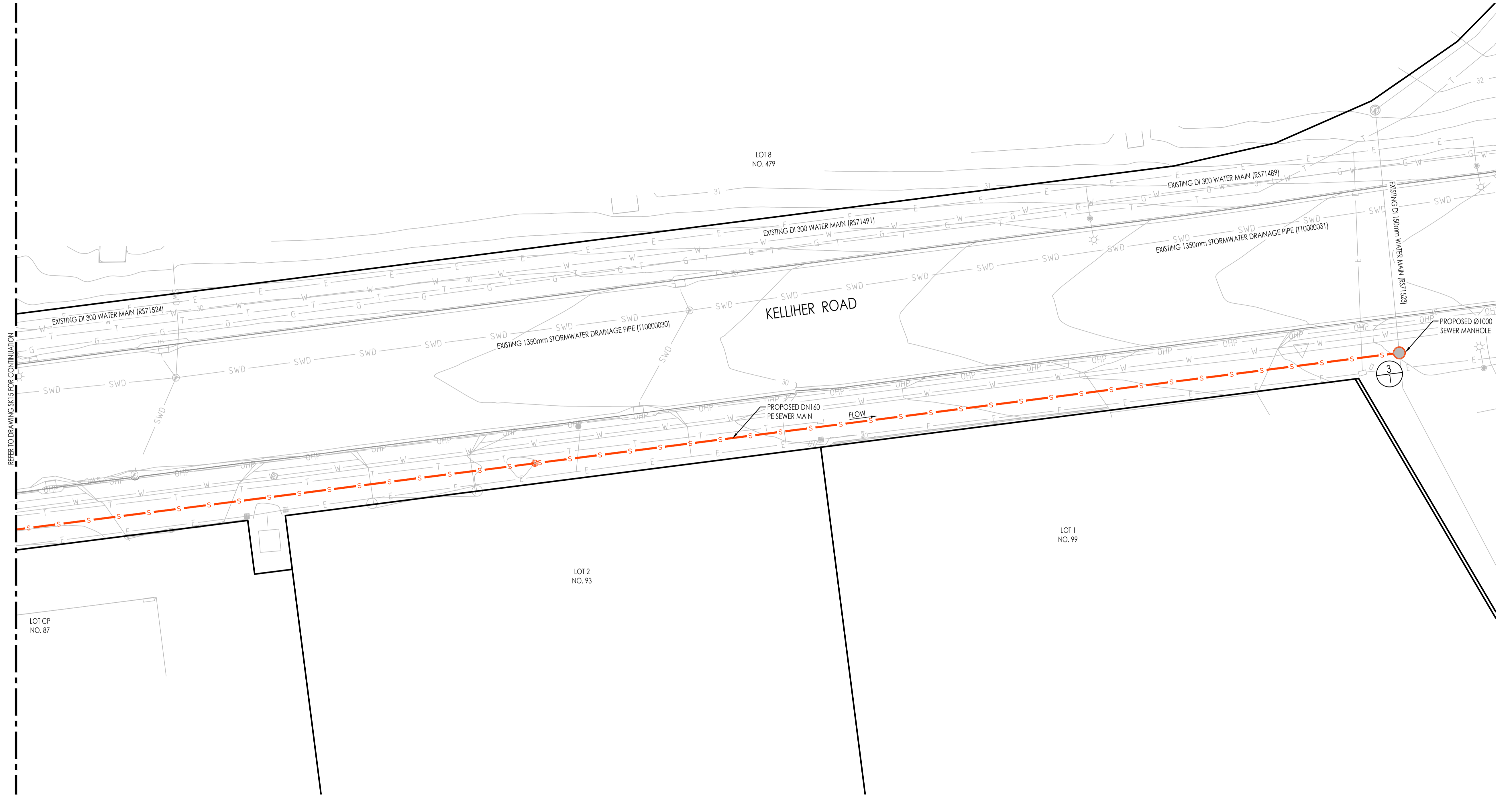
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NORTH:	RP DESCRIPTION: LOT 1 & 2 ON RP83710	CLIENT: ALSAPCE PROPERTY GROUP UNIT TRUST
APPROVED: On behalf of Rodgers Consulting Services	SCALE: 1 : 200	ORIG. DRG. SIZE A1
RPEQ. DATE:	DRAWN: KN	DESIGN: AR
	CHECKED: TS	SUPERVISOR: AR

PROJECT: 804 - 816 BOUNDARY ROAD, RICHLANDS QLD 4077	APPROVAL
TITLE: CONCEPT SEWER & WATER RETICULATION LAYOUT PLAN SHEET 1 OF 2	JOB NO. C26011
	DRAWING NO. SK15
	REVISION: A

AMENDMENTS			
NO.	DATE	DESCRIPTION	DRAWN
A	11.05.2026	PRELIMINARY ISSUE	KN





SCALE 1:200 (A1) 1:400 (A3)  
DO NOT SCALE USE ONLY THE DIMENSIONS PROVIDED.

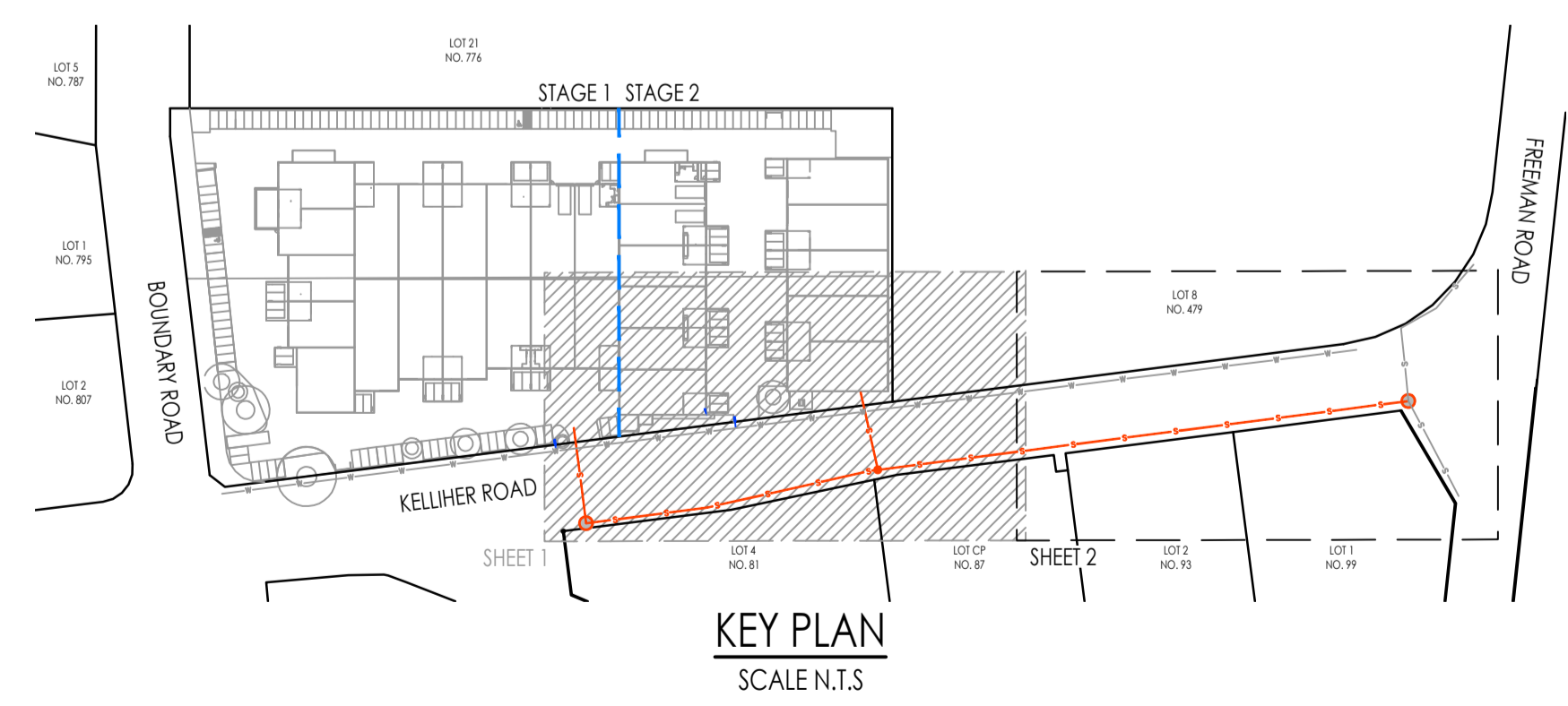
CONCEPT SEWER & WATER RETICULATION LAYOUT PLAN SHEET 2 OF 2  
SCALE 1:200

- LEGEND**
- 5.50--- EXISTING SURFACE CONTOURS
  - 5.50--- FINISHED SURFACE CONTOURS
  - PROPOSED LOT BOUNDARY
  - EXISTING LOT BOUNDARY
  - SWD EXISTING STORMWATER DRAINAGE
  - S EXISTING SEWER RETICULATION
  - W EXISTING WATER RETICULATION
  - E EXISTING UNDERGROUND POWER
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  - W PROPOSED WATER RETICULATION
  - MAINTENANCE HOLE/ SHAFT
  - B1 UPRIGHT KERB AND CHANNEL
  - SEWER STRUCTURE NUMBER
  - DRIVEWAY CROSSOVER HATCH
  - DRIVEWAY HATCH
  - STORMWATER FIELD INLET
  - STORMWATER MANHOLE
  - EASEMENT

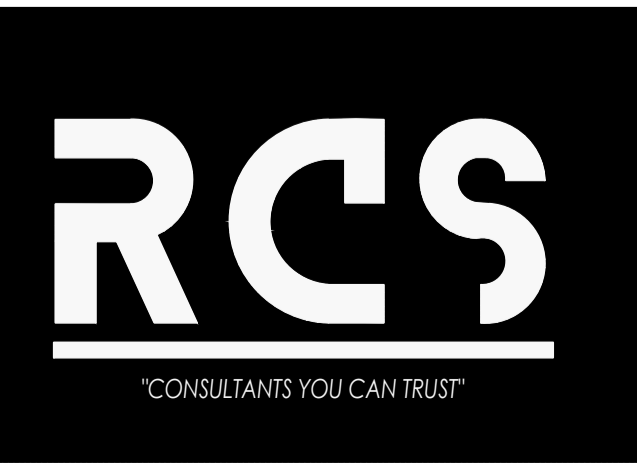
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NORTH:

RP DESCRIPTION:  
LOT 1 & 2 ON RP83710

CLIENT:  
ALSAPCE PROPERTY GROUP UNIT TRUST

SCALE: 1 : 200      ORIG. DRG. SIZE A1

APPROVED:   
On behalf of Rodgers Consulting Services

DRAWN: KN      DESIGN: AR

CHECKED: TS      SUPERVISOR: AR

RPEQ. DATE:      DATE:      DATE:

PROJECT:  
804 - 816 BOUNDARY ROAD, RICHLANDS QLD 4077

TITLE:  
CONCEPT SEWER & WATER RETICULATION LAYOUT PLAN SHEET 2 OF 2

APPROVAL	
JOB NO.	C26011
DRAWING NO.	SK16
REVISION:	A

APPENDIX D  
BCC PROPERTY LOT REPORT

**RCS**

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The property lot report shows the zone, neighbourhood plan, overlays and related information that apply to the lot on plan selected.

## Property Address

804 BOUNDARY RD RICHLANDS 4077

## Parcel Details

**Lot No and Plan:** Lot 2 on RP83710

**Full Property Holding:**

Lot 58 on RP26236

Lot 1 on RP83710

Lot 2 on RP83710

**Title Area \*:** 10,508 m<sup>2</sup>

**Ward:** DEAGON

**PDF Maps GRID Reference:** Map 42

\* refer NOTES below

[Open Cityplan.Brisbane.qld.gov.au](http://Open.Cityplan.Brisbane.qld.gov.au)



## Zones

### Name

IN3 Industry (General industry C)

### Description

The purpose of the Industry - General industry C zone code is to provide for high impact industry uses. It may include non-industrial and business uses that support the industrial activities where they do not compromise the long-term use of the land for industrial purposes. Refer to Part 6 in the City Plan 2014 and the Factsheets.

## Neighbourhood Plans

### Name

Richlands-Wacol corridor neighbourhood plan

### Description

Neighbourhood Plans provide detailed guidance for development on sites within a Neighbourhood Plan boundary. Refer to the Richlands-Wacol corridor neighbourhood plan code.

Richlands central precinct - NPP-002

Richlands central precinct - NPP-002 of the Richlands-Wacol corridor neighbourhood plan.

## Overlays

### Name

Airport environs overlay

### Description

The Airport environs overlay deals with issues of State Interest. It may also include locally identified issues that relate to airport environments. Refer to Part 8 in the City Plan 2014.

OLS - Conical limitation surface boundary

OLS – Conical limitation surface boundary sub-categories of the Airport environs overlay.

NOTE: Where development intrudes into an airport's OLS or PANS-OPS, advice from the Civil Aviation Safety Authority should be sought.

OLS - Conical limitation surface contours

OLS – Conical limitation surface contours sub-categories of the Airport environs overlay.

NOTE: Where development intrudes into an airport's OLS or PANS-OPS, advice from the Civil Aviation Safety Authority should be sought.

Procedures for air navigation surfaces (PANS)

Procedures for air navigation surfaces (PANS) sub-categories of the Airport environs overlay.

NOTE: Where development intrudes into an airport's OLS or PANS-OPS, advice from the Civil Aviation Safety Authority should be sought.

BBS zone - Distance from airport 3-8km

BBS zone - Distance from airport 3-8km sub-categories of the Airport environs overlay.

LI - Within 6km - Max intensity of light sources 3 deg above horizon

LI - Within 6km - Max intensity of light sources 3 deg above horizon sub-categories of the Airport environs overlay.

Name	Description
Bicycle network overlay	<p>The Bicycle network overlay deals with the provision of bikeway infrastructure and facilities to encourage the safe and efficient movement of pedestrians and cyclists through the movement network. Development is governed by the Bicycle network overlay code. Refer to Part 8 in the City Plan 2014.</p> <p>The Bicycle network overlay includes:</p> <ul style="list-style-type: none"><li>• Primary cycle route sub-category</li><li>• Secondary cycle route sub-category</li><li>• Local cycle route sub-category</li><li>• Riverwalk - Typology 1 (City reaches north and south) sub-category</li><li>• Riverwalk - Typology 2 (Urban reaches) sub-category</li><li>• Riverwalk - Floating walkway sub-category</li></ul>
Biodiversity areas overlay	<p>The Biodiversity areas overlay deals with biodiversity Areas of Ecological Significance (AES). This may include areas of High Ecological Significance (HES) and also areas of General Ecological Significance (GES) among others. Refer to Part 8 in the City Plan 2014.</p>
High ecological significance sub-category	High ecological significance sub-category of the Biodiversity areas overlay.
Matters of state environmental significance (MSES) sub-category	Matters of state environmental significance (MSES) sub-category of the Biodiversity areas overlay.
Matters of state environmental significance (MSES) sub-category	Matters of state environmental significance (MSES) sub-category of the Biodiversity areas overlay.
Community purposes network overlay	<p>The Community purposes network overlay implements the policy direction in the Strategic framework with respect to Brisbane's coordinated infrastructure planning and delivery, identifying land within the Community purposes network. Refer to Part 8, Part 10 Other Plans, Part 10.3.1 long term infrastructure plans for the Community Purpose Network in the City Plan 2014 and the Factsheets.</p> <p>The Community purposes network overlay includes the following sub-categories:</p> <ul style="list-style-type: none"><li>• Existing trunk park sub-category</li><li>• Existing non-trunk park sub-category</li><li>• Existing community facilities and land for community facilities sub-category</li><li>• LGIP planned land for community facilities specific location sub-category</li><li>• LGIP planned park acquisition specific location sub-category</li><li>• LGIP planned park upgrade specific location sub-category</li><li>• LGIP planned park embellishment specific location sub-category</li><li>• LGIP planned corridor park specific location sub-category</li><li>• Long term land for community facilities specific location sub-category</li><li>• Long term park specific location sub-category</li><li>• Long term corridor park specific location sub-category</li></ul> <p>Refer to the Community purposes network map to see which sub-categories are relevant to specific properties.</p> <p>For property enquiries relating to long term infrastructure contact Council via the Pre-lodgement advice service.</p>
Critical infrastructure and movement network overlay	<p>The Critical infrastructure and movement network overlay identifies critical assets and movement networks. Refer to Part 8 in the City Plan 2014.</p> <p>The Critical infrastructure and movement network overlay includes:</p> <ul style="list-style-type: none"><li>• Critical assets sub-category</li><li>• Critical infrastructure and movement planning area sub-category</li></ul> <p>Refer to the overlay map to see which sub-categories are relevant to specific properties.</p>
Critical infrastructure and movement planning area sub-category	Critical infrastructure and movement planning area sub-category of the Critical infrastructure and movement network overlay.
Industrial amenity overlay	<p>The Industrial amenity overlay provides for the assessment of the suitability of development in the Industrial amenity overlay. Development within these areas is governed by the Industrial amenity overlay code. Refer to Part 8 in the City Plan 2014.</p>
Industrial amenity investigation area sub-category	Industrial amenity investigation area sub-category of the Industrial amenity overlay.
Regional infrastructure corridors and substations overlay	<p>The Regional infrastructure corridors and substations overlay deals with electricity substations and regional infrastructure corridors for major electricity infrastructure, pipelines, regional recreation trails and stock routes. Refer to Part 8 in the City Plan 2014.</p>

Name	Description
Major electricity infrastructure high voltage powerline easement sub-category	Major electricity infrastructure high voltage powerline easement sub-category of the Regional infrastructure corridors and substations overlay.
Road hierarchy overlay	<p>The Road hierarchy overlay applies to the existing and future road networks, including state controlled roads. Refer to Part 8 and Part 10 Other Plans, Part 10.3.3 long term infrastructure plans (corridor plan) for the road network in the City Plan 2014 and the Factsheets.</p> <p>The Road hierarchy overlay includes:</p> <ul style="list-style-type: none"><li>• Motorways sub-category</li><li>• Arterial roads sub-category</li><li>• Suburban roads sub-category</li><li>• District roads sub-category</li><li>• Neighbourhood roads sub-category</li><li>• Future motorway sub-category</li><li>• Future arterial road sub-category</li><li>• Future suburban road sub-category</li><li>• Future district road sub-category</li><li>• Primary freight routes sub-category</li><li>• Primary freight access sub-category</li></ul>
Streetscape hierarchy overlay	<p>Refer to the overlay map to see which sub-categories are relevant to specific properties. NOTE: Land that adjoins land where an overlay sub-category applies, is within the overlay sub-category.</p> <p>The Streetscape hierarchy overlay identifies the various functions of the streetscape network and determines how development is assessed to ensure high quality subtropical streetscape outcomes are achieved. Refer to Part 8 in the City Plan 2014. The Streetscape hierarchy overlay includes:</p> <ul style="list-style-type: none"><li>• Subtropical boulevard - in centre verge width 6m sub-category</li><li>• Subtropical boulevard - in centre verge width 5m sub-category</li><li>• Subtropical boulevard - in centre verge width 3.75m/4.25m sub-category</li><li>• Subtropical boulevard - out of centre verge width 6m sub-category</li><li>• Subtropical boulevard - out of centre verge width 5m sub-category</li><li>• Subtropical boulevard - out of centre verge width 3.75m/4.25m sub-category</li><li>• Centre street major sub-category</li><li>• Centre street minor sub-category</li><li>• Neighbourhood street major subcategory</li><li>• Neighbourhood street minor sub-category</li><li>• Industrial street sub-category</li><li>• Pathway link sub-category</li><li>• Corner land dedication sub-category</li><li>• Locality street subcategory</li><li>• Laneway sub-category</li><li>• Wildlife movement solution sub-category</li></ul>
Transport noise corridor overlay	<p>Refer to the overlay map to see which sub-categories are relevant to specific properties. NOTE: Land that adjoins land where an overlay sub-category applies, is within the overlay sub-category.</p> <p>The Transport noise corridor overlay deals with areas of land identified as being affected by transport noise as established under Chapter 8B of the Building Act 1975. It may include areas of land affected by noise from:</p> <ul style="list-style-type: none"><li>• State controlled roads (State mapping)</li><li>• Franchised roads</li><li>• Local government controlled roads</li><li>• Railway land (State mapping)</li></ul>
Designated State Noise corridor - State controlled road (MANDATORY area): Category 0: Noise Level < 58 dB(A)	Designated State Noise corridor - State-controlled road (MANDATORY area): Category 0: Noise Level < 58 dB(A) of the Transport noise corridor overlay.

Name	Description
Designated State Noise corridor - State controlled road (MANDATORY area): Category 1: 58 dB(A) - 63 dB(A)	Designated State Noise corridor - State-controlled road (MANDATORY area): Category 1: 58 dB(A) - 63 dB(A) of the Transport noise corridor overlay.
Designated State Noise corridor - rail network: Category 0: Noise level <70 dB(A)	Designated State Noise corridor - rail network: Category 0: Noise level <70 dB(A) of the Transport noise corridor overlay.

## Local Government Infrastructure Plan

Name	Description
INCLUDED in Priority Infrastructure Area Note. - some properties may be only partly included in the Priority Infrastructure Area.	The priority infrastructure area identifies the areas that the local government prioritises in order to provide trunk infrastructure for urban development. The purpose of the priority infrastructure area is to align the footprint for development with the plans for trunk infrastructure. LGIP maps are referenced in Part 4 of City Plan 2014. Local Government Infrastructure Plan mapping and support material are in Schedule 3 of City Plan 2014. Refer to Factsheets.
Plans for Trunk Infrastructure (PFTI) PFTI Map Grid Reference Map Grid 290 All networks applicable	All Networks. The Plans for Trunk Infrastructure maps (Schedule 3) have been grouped by map tile. Please also refer to the map indexes relevant to each of the networks: Transport network (pathway network and ferry terminals network) maps; Public parks and land for community facilities network maps; Transport network (road network) maps; Stormwater network maps; NOTE: The water supply network and sewerage network related information is now included in Queensland Urban Utilities (QUU) water netserv plan. Further details can be obtained from QUU.

## Other Plans

Name	Description
Stormwater network	<p>The Stormwater Code and the Long term infrastructure plans Other plan map for the Stormwater network implements the policy direction in the Strategic framework with respect to a coordinated infrastructure planning and delivery, identifying land within the Stormwater network. Refer to Part 9, Part 10 Other Plans, Part 10.3.2 long term infrastructure plans for the Stormwater network in the City Plan 2014 and the Factsheets.</p> <p>The Long term infrastructure plans Other plan map for the Stormwater network includes the following types of items:</p> <ul style="list-style-type: none"><li>• Bioretention swale</li><li>• Land</li><li>• Natural channel</li><li>• Pipe (new)</li><li>• Pipe (relief drainage)</li><li>• Culvert</li><li>• Stormwater quality improvement device</li><li>• Rehabilitation</li><li>• Backflow prevention device</li></ul> <p>Refer to the Other plan map for Stormwater network to see which items are relevant to specific properties.</p> <p>For property enquiries relating to long term infrastructure contact Council via the Pre-lodgement advice service.</p>


Regard must be had to the *Brisbane City Plan 2014* when interpreting this property report (*this Report*). Some information relating to overlays and neighbourhood plans may not be shown in the Report.

**NOTES**

- a) Areas shown in this Report are approximate only.
- b) Contour information shown is from Council's 2002 Contour records.
- c) Further information on mining tenements issued under the Mineral Resources Act 1989 can be obtained from the Queensland State Government.
- d) A [Temporary Local Planning Instrument \(TLPI\)](#) may affect a particular property. TLPIs are not identified in this report. Visit the [Temporary Local Planning Instrument page](#) at [www.Brisbane.qld.gov.au](http://www.Brisbane.qld.gov.au) to confirm whether this property is included in a TLPI.
- e) Users of the information recorded in this document (the **Information**) accept all responsibility and risk associated with the use of the Information and acknowledge that regard must be had to the planning scheme provisions in interpreting the Information. Council gives no warranty in relation to the Information (including accuracy, reliability, completeness, currency or suitability) and accepts no liability (including without limitation, liability in negligence) for any loss, damage or costs (including consequential damage), relating to any use of this Information.

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**Local Government Authorities**

 LGA boundary

**[Superseded] City Plan 2014 — v31.00–2024 — Property boundaries — Holding**

 Property Holding

APPENDIX E  
EROSION HAZARD ASSESSMENT

**RCS**

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# Erosion Hazard Assessment

Brisbane City Council (BCC), *Erosion Hazard Assessment* form must be read in conjunction with the *Erosion Hazard Assessment- Supporting Technical Notes* (June 2014 or later version) for explanatory terms and Certification information.

## What is an Erosion Hazard Assessment?

Soil erosion and sediment from urban development, particularly during construction activities, is a significant source of sediment pollution in Brisbane's waterways. The Erosion Hazard Assessment determines whether the risk of soil erosion and sediment pollution to the environment is 'low', 'medium' or 'high'.

## When is the EHA required?

An Erosion Hazard Assessment form must be completed and lodged with BCC for any Development Application (ie MCU or ROL) that will result in soil disturbance OR Operational Works or Compliance Assessment Application for 'Filling' or Excavation.

**Failure to submit this form during lodgement of an application may result in assessment delays or refusal of the application.**

## Privacy Statement

The personal information collected on this form will be used by Brisbane City Council for the purposes of fulfilling your request and undertaking associated Council functions and services. Your personal information will not be disclosed to any third party without your consent, unless this is required or permitted by law.

## Assessment Details

1 Please turn over and complete the erosion hazard assessment.

2 Based on the erosion hazard assessment overleaf, is the site:

**A 'low' risk site**

*Best practice erosion and sediment control (ESC) must be implemented but no erosion and sediment control plans need to be submitted with the development application. Factsheets outlining best practice ESC can be found at <https://waterbydesign.com.au/download/erosion-sediment-control-for-small-construction-sites>*

**A 'medium' risk site**

*If the development is approved, the applicant will need to engage a Registered Professional Engineer (RPEQ) or Certified Professional in Erosion and Sediment Control (CPESC) to prepare an ESC Program and Plan and supporting documentation — in accordance with the requirements of the Infrastructure Design Planning Scheme Policy.*

**A 'high' risk site**

*If the development is approved, the applicant will need to engage a RPEQ and CPESC to prepare an ESC Program and Plan and supporting documentation — in accordance with the requirements of the Infrastructure Design Planning Scheme Policy. The plans and program will need to be certified by a CPESC.*

## 3 Site Information and Certification

Application number (if known)

Site address

804-816 Boundary

Richalnds QLD

Postcode 4077

I certify that:

- I have made all relevant enquiries and am satisfied no matters of significance have been withheld from the assessment manager.
- I am a person with suitable qualifications and/or experience in erosion and sediment control.
- The Erosion Hazard Assessment was completed in accordance with the Erosion Hazard Assessment Supporting Technical Notes and the BCC Infrastructure Design Planning Scheme Policy.
- The Erosion Hazard Assessment accurately reflects the site's overall risk of soil erosion and sediment pollution to the environment.
- I acknowledge and accept that the BCC, as assessment manager, relies, in good faith, on this certification as part of its development assessment process and the provision of false or misleading information to the BCC constitutes an offence for which BCC may take punitive steps/ action against me/ enforcement action against me.

Certified by (Print name)

Certifier's signature

Date

11 / 05 / 2026

## Assessment Table

**Table 1: Low Risk Test**

		<b>Yes</b>	<b>No</b>
<b>1.1</b>	is the area of land disturbance > 1000 m <sup>2</sup> ?	<input checked="" type="checkbox"/>	<input type="checkbox"/>
<b>1.2</b>	does any land disturbance occur in a BCC mapped waterway corridor?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<b>1.3</b>	is there any slope on site (longer than three metres in length) before, during or after construction that is steeper than 5%?	<input checked="" type="checkbox"/>	<input type="checkbox"/>
<b>1.4</b>	does any land disturbance occur below 5 m AHD?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<b>1.5</b>	does development involve endorsement of a staging plan?	<input checked="" type="checkbox"/>	<input type="checkbox"/>
<b>1.6</b>	is there an upstream catchment passing through the site > 1 hectare?	<input checked="" type="checkbox"/>	<input type="checkbox"/>

If you answered '**No**' to **ALL** of these questions, then the site is **low risk** with respect to erosion and sediment control.  
(Do not continue to Table 2)

If you answered '**Yes**' to **ANY** of these questions, then proceed to **Table 2**

**Table 2: Medium Risk Test**

		<b>Yes</b>	<b>No</b>
<b>2.1</b>	is the area of land disturbance > 1 hectare?	<input checked="" type="checkbox"/>	<input type="checkbox"/>

If '**No**' then the site is **medium risk** with respect to erosion and sediment control.  
(Do not continue to Table 3)

If '**Yes**' then proceed to **Table 3**

**Table 3: High Risk Test**

		<b>Yes</b>	<b>No</b>
<b>3.1</b>	is there an upstream catchment passing through the site > 1 hectare?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<b>3.2</b>	does any land disturbance occurs in a BCC mapped waterway corridor?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<b>3.3</b>	is there any slope on site (longer than three metres in length) before, during or after construction that is steeper than 15%?	<input type="checkbox"/>	<input checked="" type="checkbox"/>

If you answered '**No**' to **ALL** of these questions, then the site is also **medium risk** with respect to erosion and sediment control.

If you answered '**Yes**' to **ANY** of these questions, then the site is **high risk** with respect to erosion and sediment control.

APPENDIX F  
FLOODWISE PROPERTY REPORT

**RCS**

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# FloodWise Property Report

816 BOUNDARY RD, RICHLANDS 4077  
Lot 1 on RP83710, Lot 2 on RP83710



Dedicated to a better Brisbane


## THE PURPOSE OF THIS REPORT IS FOR BUILDING AND DEVELOPMENT

Brisbane City Council's FloodWise Property Report provides technical flood planning information including estimated flood levels, habitable floor level requirements and more. This report uses the adopted flood planning information in Brisbane City Plan 2014, that guides how land in Brisbane is used and developed for the future. Find out more about [planning and building](#). To understand how to be resilient and prepare for floods, visit Council's [Be Prepared](#) webpage. Find more information about [how to read a FloodWise Property Report](#).

### This property has no flood levels

Brisbane City Council has not assigned flood level information for this property however it may be affected by one or more flood or property development flags. Please refer to the Flood Planning and Development Information below for details. The property may have 0.2% AEP flood level which will appear on the Flood Planning Information table if applicable. For professional advice or detailed assessment of a property contact a Registered Professional Engineer of Queensland.

Visit the [Be Prepared](#) page to find more information on how to prepare your home or business for potential flooding.

 **Combined** 1% AEP for river, creek and storm tide flood extent (if applicable) from the adopted Brisbane City Plan 2014. Read more about [Brisbane City Plan 2014](#).



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## Are you resilient and ready for flood?

- Sign up to the Brisbane Severe Weather Alert at [brisbane.qld.gov.au/beprepared](https://brisbane.qld.gov.au/beprepared)
- Visit [bom.gov.au](https://bom.gov.au) for the latest weather updates.
- Have an evacuation plan, emergency kit and important phone numbers ready.
- Observe where water flows from and to during heavy rain.
- Consider how flood-resilient building techniques will have you home faster and with less damage.

Life threatening emergencies  
**000** Police/fire/ambulance  
(mobiles **000** and **112**)

State Emergency Service (SES) **132 500**  
Energex **13 19 62**  
Brisbane City Council **3403 8888**

### Technical Summary

This section of the FloodWise Property Report contains more detailed flood information for this property so **surveyors, builders, certifiers, architects, and engineers can plan and build** in accordance with Council's planning scheme.

Find more information about [planning and building](#) in Brisbane or talk to a Development Services Planning Information Officer via Council's Contact Centre on (07) 3403 8888.

## Flood Planning and Development Information

This section of the FloodWise Property Report contains information about Council's planning scheme overlays. Overlays identify areas within the planning scheme that reflect distinct themes that may include constrained land and/or areas sensitive to the effects of development.

### Flood overlay code

The Flood overlay code of Council's planning scheme uses the following information to provide guidelines when developing properties. The table below summarises the flood planning areas (FPAs) that apply to this property. Development guidelines for the FPAs are explained in [Council's planning scheme](#).

Flood planning areas (FPA)		
River	Creek / waterway	Overland flow
		Not Applicable

To find more information about Council's flood planning areas (FPAs) for Brisbane River and Creek/waterway flooding to guide future building and development in flood prone areas, please review [Council's Flood Planning Provisions](#).

### Coastal hazard overlay code

The Coastal hazard overlay code of Council's planning scheme uses the following information to provide guidelines when conducting new developments. The table below summarises the coastal hazard categories that apply to this property. Development guidelines for the following Coastal hazard overlay sub-categories are explained in Council's [planning scheme](#).

Coastal hazard overlay sub-categories
There are currently no Coastal hazard overlay sub-categories that apply to this property.

Note: Where land is identified within one or more flood planning areas on the Flood overlay or is identified within one of the Storm tide inundation area sub-categories on the Coastal hazard overlay, the assessment criteria that provides the highest level of protection from any source of flooding applies.

### Property development flags

**Large allotment** - This property is either a Large Allotment of over 1000 square metres or is located within a Large Allotment. Flood levels may vary significantly across allotments of this size. Further investigations may be warranted in determining the variation in flood levels and the minimum habitable floor level across the site.

For more information or advice, please consult a Registered Professional Engineer of Queensland (RPEQ).

## Useful Flood Information Definitions

**Australian Height Datum (AHD)** - The reference level for defining ground levels in Australia. The level of 0.0m AHD is approximately mean sea level.

**Annual Exceedance Probability (AEP)** - The probability of a flood event of a given size occurring in any one year, usually expressed as a percentage annual chance.

- **0.2% AEP** - A flood event of this size is considered rare but may still occur. A flood of size or larger has a 1 in 500 chance or a 0.2% probability of occurring in any year.
- **1% AEP** - A flood of this size or larger has a 1 in 100 chance or a 1% probability of occurring in any year.
- **2% AEP** - A flood of this size or larger has a 1 in 50 chance or a 2% probability of occurring in any year.
- **5% AEP** - A flood of this size or larger has a 1 in 20 chance or a 5% probability of occurring in any year.
- **20% AEP** - A flood of this size or larger has a 1 in 5 chance or a 20% probability of occurring in any year.

### Data quality

- **Data Quality Code A** - Level data based on recent surveyor report or approved as-constructed drawings.
- **Data Quality Code B** - Level data based on ground-based mobile survey or similar.
- **Data Quality Code C** - Level data derived from Airborne Laser Scanning or LiDAR information.

**Defined Flood Level (DFL)** - The DFL is used for commercial and industrial development. The Defined flood level (DFL) for Brisbane River flooding is a level of 3.7m AHD at the Brisbane City Gauge based on a flow of 6,800 m/s. DFL is only applicable for non-residential uses affected by Brisbane River flooding.

**Flood planning area (FPA)** - Council has developed five Flood planning areas (FPAs) as part of Brisbane City Plan 2014 Flood overlay mapping for Brisbane River, Creek/waterway flooding and Overland flow to guide future building and development in flood prone areas. Storm tide flooding is mapped separately. The FPAs are designed to recognise the flood hazard for different flooding types. Flood hazard is a combination of frequency of flooding, the flood depth, and the speed at which the water is travelling. [Find more information here.](#)

**Maximum and minimum ground level** - Highest and lowest ground levels on the property based on available ground level information. A Registered Surveyor can confirm exact ground levels.

**Minimum habitable floor level (dwelling house)** - The minimum level in metres AHD at which habitable areas of development (generally including bedrooms, living rooms, kitchen, study, family, and rumpus rooms) must be constructed as required by the Brisbane City Plan 2014.

**Indicative existing floor level** - The approximate level in metres AHD of the lowest habitable floor in the existing building (excluding apartments). The data is sourced from a range of sources with varying accuracy levels.

**Property** - A property will contain 1 or more lots. The multiple lot warning is shown if you have selected a property that contains multiple lots.

**Residential flood level (RFL)** - This flood level for the Brisbane River equates to the 1% annual exceedance probability (AEP) flood level.

To learn more, visit [Brisbane City Council's Flood Information Hub](#)

## Brisbane City Council's Online Flood Tools

Council provides several online flood tools:

- to guide planning and development
- to help residents and businesses understand their flood risk and prepare for flooding.

Council's online flood tools for planning and development purposes include:

- **FloodWise Property Report**
- **Flood Overlay Code**

For more information on Council's planning scheme and online flood tools for planning and development:

- phone (07) 3403 8888 and ask to talk to a Development Services Planning Information Officer

- visit [brisbane.qld.gov.au/planning-building](https://brisbane.qld.gov.au/planning-building)

Council's Planning Scheme - The Brisbane City Plan 2014 (planning scheme) has been prepared in accordance with the Sustainable Planning Act as a framework for managing development in a way that advances the purpose of the Act. In seeking to achieve this purpose, the planning scheme sets out the Council's intention for future development in the planning scheme area, over the next 20 years.

### Disclaimer

1. Defined flood levels and residential flood levels, minimum habitable floor levels and indicative existing floor levels are determined from the best available information to Council at the date of issue. These levels, for a particular property, may change if more detailed information becomes available or changes are made in the method of calculating levels.
2. Council makes no warranty or representation regarding the accuracy or completeness of a FloodWise Property Report. Council disdaims any responsibility or liability in relation to the use or reliance by any person on a FloodWise Property Report.



### Planning to build or renovate?

For information, guidelines, tools and resources to help you track, plan or apply for your development visit [brisbane.qld.gov.au/planning-building](https://brisbane.qld.gov.au/planning-building)

You can also find the Brisbane City Plan 2014 and Neighbourhood Plans as well as other information and training videos to help, with your building and development plans.