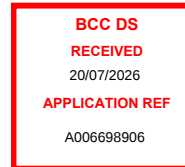


06 July 2026

Enquiries: Jason Bentley & Alex Saunders
Project No: 301050923

Brisbane City Council
266 George Street
GPO Box 1434
Brisbane Qld 4000



Attention: Shirley Mills

**RE: 341 Beckett Road Bridgeman Downs
Information Request Response (A006698906)**

Please find enclosed our response package to the Brisbane City Council Information Request email, dated 15th April 2025.

The civil engineering documentation has been updated to reflect the elements requested in the information request. A specific response to the items raised are listed below:

ORDERLY DEVELOPMENT

- 1) Council is concerned that the proposal is out-of-sequence as it relies upon the road corridor and stormwater infrastructure to be provided by 357 and 365 Beckett Road (Lot 6 on RP 93687 and Lot 5 on SP227448) or 113 and 117 Idonia Street (Lot 10 on RP93688 and Lot 9 on RP93688) for access to all lots. Whilst Council is currently assessing development applications for 113 and 117 Idonia Street, and notwithstanding there is a current approval over 357 and 365 Beckett Road, there is no certainty that these developments will ultimately go ahead. This issue will require further interrogation and understanding. More specific issues with respect to sequencing is detailed below.

Response: Refer to response by Saunders Havill Group.

ROAD DESIGN

- 2) The proposed road design is generally acceptable. However, there is uncertainty in relation to the proposed road grades and expected alignment with future road connections to the north and east.
 - a) Provide preliminary road design plans, endorsed by an RPEQ in accordance with PO12 of the Subdivision code, showing cross sections as well as longitudinal grades. The design is also to consider adjacent developments and future road connections, clearly showing levels and sections to ensure transition between adjacent sites can be achieved.

Response: Refer to enclosed civil engineering plans for details including the indicative long sections for road connections to adjacent properties to the North and East. Note that these road long sections are subject to further design progression under an OPW phase and coordination with adjacent developments.

FILLING AND EXCAVATION

- 3) The development site falls from the southeastern corner of the site to the south and south

Design with community in mind

west. There is significant fall across the site from contour 47 to 39 at its lowest point.

In an effort to achieve level pads substantial cut is proposed for lots 6 to 15, in some cases in excess of 3m, with a combination of batters and retaining walls. The proposed batters exceed the maximum accepted 1 in 4 grade.

For Lots 16 to 19 split pad levels are proposed with the explanation that future dwellings are expected to be constructed as "split" level dwellings. There are no dwelling designs attached to these specific lots as part of this application. As such, the proposed Lots 16 to 19 do not meet earthworks requirements of PO2 of the Subdivision code.

It is also noted that retaining proposed in fill between lots 19, 21 and 22 exceeds the maximum 1m height at the property boundary.

- b) Submit a revised earthworks plan, endorsed by an RPEQ including the following:
- All retaining structures in fill at property boundaries (includes new lots) at a maximum 1m height and all further terracing from the 1m high boundary retaining wall is 1 vertical unit:1 horizontal unit. Where a performance outcome is sought it is to be clearly identified and justified by an RPEQ.
 - All Batters no greater than 1 in 4 grades
 - Additional sections from Lot 15 and Lot 16 to the east taking into consideration proposed developments at 117 and 113 Idonia Street. All new retaining and existing levels are to be shown.
 - Lots 16 to 19 earthworks in accordance with PO2 of the Subdivision code. Alternatively, provide specific dwelling designs that are to be constructed and delivered along with the specified lots.
 - Cut/fill legend that accurately reflects colours shown on plan.

Response: Refer to enclosed civil engineering plans for revised layout and earthworks/retaining wall strategy addressing the above with below comments.

- i) *Retaining walls along road frontages have been maintained at a 1m height, however, due to the existing grade of the site multiple retaining walls are required to exceed 1m in height to avoid significant impact on development. It is noted that single tier retaining walls between proposed allotments have been restricted to 2.0m in height, with multi-tier retaining walls being restricted to 1.5m in height within a single tier.*

Where retaining walls exceed 1.0m, either as a single tier or multi-tier wall, the allotments are configured so that the retaining walls are typically positioned on the Southern or Eastern boundary of the lower allotment. This has been done to reduce impacts to the amenity and perceived imposition on the lower allotment resulting from the retaining walls.

Additionally, retaining walls along the Western boundary of Lots 12-15 are located adjacent to driveways which provide shared access to Lots 9-11 and 12-14. This further reduces the impact of these retaining walls on the lower allotments.

- ii) *Batters are generally 1in4 to 1in6 except to Lots 1-4 and 16-19 where the mid-block batters are maintained at 1in3 as part of the proposed works as an increase to 1in4 would impact the proposed environmental protection zone.*
- iii) *Refer to enclosed sections noting that these are to the existing levels of the adjacent properties. Further coordination of levels and interfaces between developments will be required as part of OPW phases.*
- iv) *Refer to enclosed indicative dwelling design plans for sites 16-19*
- v) *Refer to enclosed civil engineering plans*

STORMWATER

- 4) The Stormwater Management Technical Memo submitted with the application acknowledges the existing overland flow path however, provides little comment on how proposed earthworks on Lots 1 to 4 and filling of the existing dam will impact on the existing overland flow path that flows onto the neighbouring property to the north.
- a) Provide an updated stormwater report addressing any impacts on the existing overland flow path resulting from proposed earthworks on site. The report is also to identify and show the extent of the easement over the overland flow path. The report must be endorsed by a RPEQ.

Response: *The existing overland flow path is generally retained with stormwater flows discharging, from Becketts Road, through Lot 5 and into the proposed road development road network towards Endell Street to the North. A bund has been introduced along the Northern boundary to ensure that any addition flows from the Lot 4 portion of Beckett Road and from Lots 1-3 are directed towards the proposed roadway which effectively maintains the existing overland flow path.*

An updated Stormwater Technical Memo has not been provided as the proposed outcomes are consistent with the memo dated 17 January 2025

LAWFUL POINT OF DISCHARGE

- 5) The proposed development is dependent on approved development A006180740 to the north to be constructed and accepted on maintenance for the proposed design to effectively achieve a lawful point of discharge (LPD). Should the proposed development to the north not be enacted, this development will need to identify a LPD.
- b) Provide further clarification with respect to how the site can achieve LPD in accordance with PO3 of the Stormwater code.

Response: *Refer to response by Saunders Havill Group regarding development order.*

STORMWATER QUALITY TREATMENT

- 6) The proposed treatment train, comprising entirely of tree pits and bio retention pods would not be effective in servicing such a large development and is not supported. The proposal introduces potential obstructions to lot access. The proposed development has not demonstrated water quality can be achieved in accordance with PO1 and PO6 of the Stormwater code.
- a) Submit a revised stormwater report and water quality treatment with a bio retention basin within future Council land. The report is required to be endorsed by a RPEQ.

Response:

No changes have been made to the proposed usage of stormwater quality devices for the project with a combination of bio-pods and tree pits remaining the preferred treatment outcome.

It is noted that it is not considered a good outcome for a traditional basin to be situated above residential properties which would be required in this instance due to the requirement to achieve the road connection to that proposed by the Northern development.

Additionally, the proposed developments on adjacent allotments (e.g. 365 Becketts Road (A006180740),

113 Idonia St (A006724561), 117 Idonia St (A006688153), similarly have stormwater quality treated via the usage of tree pits and/or a combination of bio-pods and tree pits with developments being similar in size for proposed allotments and having similar constraints.

An updated Stormwater Technical Memo has not been provided as the proposed outcomes are consistent with the memo dated 17 January 2025

UTILITIES

- 7) It appears that the proposed development is dependent on surrounding developments to achieve serviceability. This also appears to apply to other services such as electricity, telecommunications, water and sewerage.

a) Provide a SAN notice from Urban Utilities to demonstrate that the development can be provided with water and sewerage infrastructure in accordance with PO4 of the Subdivision code.

Response: *Serviceability will be provided by continuation of infrastructure in adjacent developments, in the same way as road access and stormwater. Engagement with the various utility providers will occur post development approval.*

REFUSE COLLECTION VEHICLE TURNAROUND AND ROAD PAVEMENT

- 8) It is noted that a temporary turnaround facility is proposed for Lot 17 separate to the lot's vehicle crossover however, the proposed gradient of 1:9.7 as shown in Section C on drawing number CI-DA-103-C01 (Rev A, 20/12/24 by Stantec) exceeds the maximum 1:20 allowable for refuse collection vehicle (RCV) manoeuvring.

This is not safe or functional and does not comply with AO4.1 and AO9 of the Subdivision code and AO8.1 of the Infrastructure design code. Further, the road pavement and associated kerb and channel appears to stop short of the eastern boundary preventing the RCV from undertaking safe manoeuvring and refuse collection for proposed Lot 15.

Additionally, it is noted that the Concept Plan by Saunders Havill Group (drawing number 12152 P 03, Rev C, 22/11/24) has not demonstrated vehicle crossovers for the proposed lots as shown on the drawings by Stantec which is required to undertake further assessment and to ensure consistency across all drawings.

- a) Submit amended plans that demonstrate:
- A maximum 1:20 gradient for all RCV manoeuvring areas including turnaround facilities;
 - Road pavement extended to the eastern boundary;
 - Vehicle crossovers for proposed lots 3, 4, 5, 6 and 17 consistent with those shown on civil drawings by Stantec.
- b) Utilising amended plans, provide RPEQ certified side loading RCV swept path analysis that demonstrates safe and efficient manoeuvring and that the RCV's side arm can service the frontage of each lot (Lot 3 excepted). Refer to Refuse planning scheme policy Table 3 for side loading RCV specifications.

Response:

- a) *Refer to enclosed civil engineering plans for details for updated drawings containing the requested*

information.

- i. Gradient to temporary refuse collection manoeuvring driveway is circa 2.5% noting that refuse collection driveway will be subject to OPW design.
 - ii. Road pavement has been extended to approximately 1.0m from Eastern boundary to allow for constructability of road.
 - iii. Refer to SHG plans noting changes in development configuration and layout.
- b) Refer to enclosed civil engineering plans for revised layout, noting that proposed driveway positions for Lot 15 is circa 10m from the proposed Eastern boundary which enable collection to the left hand side of the RCV while the Western end of Road 2 is a standard BCC 'T' head.

BIN PADS

- 9) Concrete bin pads have been demonstrated on civil drawings by Stantec (drawing numbers CI-DA-080-P01 and CI-DA-400-P01).

These are not in accordance with the requirements in City Plan 2014 and are therefore, not required or supported.

- a) Submit amended plans that remove all concrete bin pads and references to "indicative bin pads".

Response: Refer to enclosed civil engineering plans. All bin pads have been removed.

ECOLOGICAL VALUES

Response: Refer to separate response by Saunders Havill Group.

Should you have any queries with the enclosed responses, please don't hesitate to call.

Yours sincerely,

Stantec Australia Pty Ltd



Jason Bentley
Senior Civil Engineer (RPEQ: 26711)

Encl:

Attachment 1 – Civil Engineering drawings

Attachment 2 – Example Terraced Building Design



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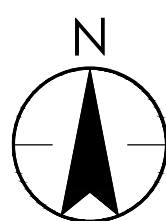
Email: enquiries.bri@stantec.com www.stantec.com



Attachment 1 – Civil Engineering drawings

STEVE BROWN CONSTRUCTION PTY LTD & MEI WAH WONG

341-349 BECKETT ROAD, BRIDGEMAN DOWNS



SITE LOCALITY PLAN
SOURCE: Nearmap 2024

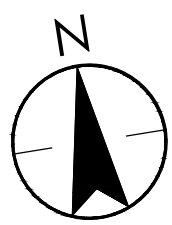
DRAWING LIST	
NO.	DRAWING NAME
CI-DA-000-P01	COVER SHEET
CI-DA-080-P01	CONCEPTUAL SERVICES COORDINATION PLAN
CI-DA-100-P01	CONCEPTUAL EARTHWORKS LAYOUT PLAN
CI-DA-100-P02	CONCEPTUAL EARTHWORKS SHADING PLAN
CI-DA-103-C01	EARTHWORKS SECTIONS
CI-DA-106-D01	EARTHWORKS DETAILS
CI-DA-400-P01	ROADWORKS & STORMWATER LAYOUT PLAN
CI-DA-400-L01	ROADWORKS LONGITUDINAL SECTION MC01
CI-DA-400-L02	ROADWORKS LONGITUDINAL SECTION MC02
CI-DA-400-C01	ROADWORKS CROSS SECTIONS MC01
CI-DA-400-C02	ROADWORKS CROSS SECTIONS MC02

PRIOR TO COMMENCEMENT OF NEW WORK THE CONTRACTOR SHALL CONFIRM LOCATIONS, LEVELS AND DETAILS OF EXISTING CONNECTION POINTS AND SERVICE CROSSINGS BY POT HOLING. IF A VARIATION OCCURS CONTACT THE ENGINEER PRIOR TO CONSTRUCTION.



03/07/2026

PROJECT NUMBER: 301050923



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B	IR RESPONSE	SSBJ	JB 2026.07.07
A	CONCEPT DESIGN	JB	JB 2024.12.20
Issued/Revision		By	Appd YYYY.MM.DD
File Name: 301050923-CL-DA-080-P01.DWG		DDJ	LZ/KJ HW/JB
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Issue Status

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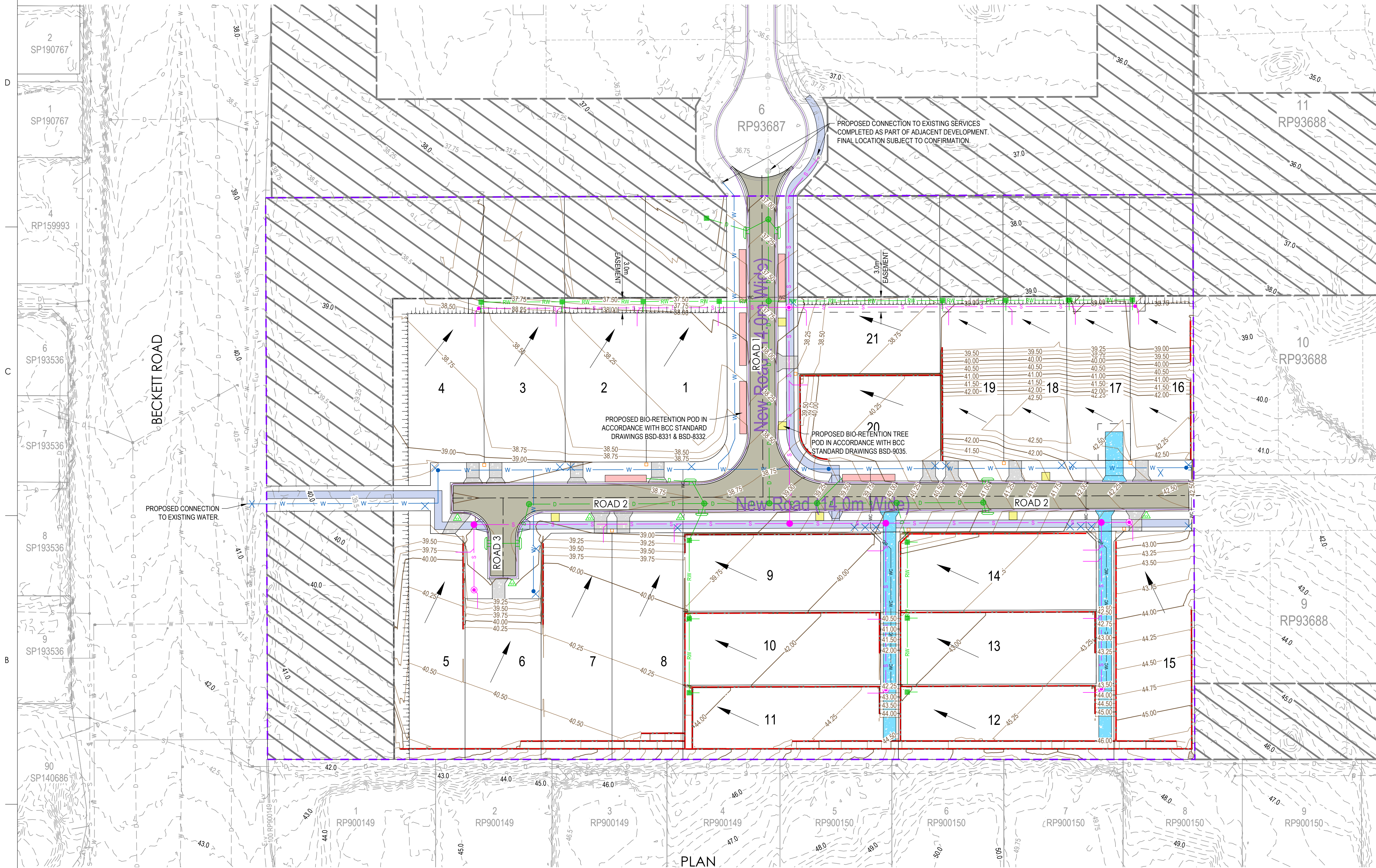
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341-349 BECKETT ROAD, BRIDGEMAN DOWNS

SUBDIVISION DEVELOPMENT APPLICATION

Title CONCEPTUAL SERVICES COORDINATION PLAN

Project No. 301050923	Scale 1:400 (A1)
Revision B	Drawing No. CI-DA-080-P01



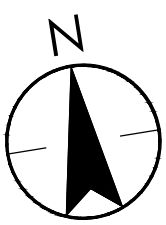
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- ALL LOTS SHALL BE PROVIDED WITH CONNECTION TO SEWER, WATER, ELECTRICITY, TELECOMMUNICATIONS AND A LAWFUL POINT OF STORMWATER DISCHARGE.
- WHERE EXISTING DWELLINGS ARE BEING RETAINED AS PART OF THE PROPOSAL, THE EXISTING DWELLING IS TO BE RECONNECTED TO ALL SERVICES UPON COMPLETION OF THE WORKS INCLUDING DECOMMISSIONING OF THE EXISTING SEPTIC TANK IF APPLICABLE.

LEGEND

	SITE BOUNDARY		PROPOSED ROAD CONTROL LINE		PROPOSED STORMWATER LINE
	EXISTING MAJOR CONTOURS & LABEL (1.0m INTERVAL)		PROPOSED RETAINING WALL		PROPOSED ROOFWATER LINE
	EXISTING MINOR CONTOURS & LABEL (0.25m INTERVAL)		PROPOSED MOUNTABLE KERB & CHANNEL		PROPOSED KERB ADAPTOR /STORMWATER GULLY / MANHOLE / PIT
	PROPOSED MAJOR CONTOURS & LABEL (1.0m INTERVAL)		PROPOSED BATTER TOP		PROPOSED BIORETENTION POD
	PROPOSED MINOR CONTOURS & LABEL (0.25m INTERVAL)		PROPOSED CONCRETE DRIVEWAY - BY DEVELOPER		PROPOSED WSUD STREET TREE POD
	ENVIRONMENTAL PROTECTION ZONE		INDICATIVE CONCRETE DRIVEWAY - BY OWNER/BUILDER		PROPOSED SEWER LINE
	PROPOSED EASEMENT BOUNDARY		PROPOSED CONCRETE FOOTPATH		PROPOSED SEWER HOUSE CONNECTION / MS / MANHOLE / IOS
	ADJACENT SITE - INDICATIVE KERB & CHANNEL		PROPOSED NEW PAVEMENT		PROPOSED WATER LINE
	ADJACENT SITE - STORMWATER LINE		INDICATIVE BIN PAD		PROPOSED WATER CONDUIT
	ADJACENT SITE - SEWER LINE		EXISTING STORMWATER LINE		PROPOSED WATER END CAP / VALVE / FH / METER
	ADJACENT SITE - WATER LINE		EXISTING SEWER LINE		PROPOSED POWER PILLAR
	PROPOSED SURFACE FLOW DIRECTION		EXISTING WATER LINE		
			EXISTING LV POWER LINE		





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Notes

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A	CONCEPT DESIGN	JB	JB	2024.12.20	
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SUBDIVISION DEVELOPMENT APPLICATION

Title
CONCEPTUAL EARTHWORKS LAYOUT PLAN

Project No. 301050923	Scale 1:400 (A1)
Revision B	Drawing No. CI-DA-100-P01

EARTHWORKS NOTES

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- ALL RETAINING WALLS PROPOSED ARE TO BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE BCA AND THE LOCAL AUTHORITY.
- ALL PROPOSED EARTHWORKS SHALL BE DESIGNED AND CONSTRUCTED IN ACCORDANCE WITH THE REQUIREMENTS OF THE RELEVANT AUTHORITY SPECIFICATIONS FOR FILLING AND EXCAVATION.

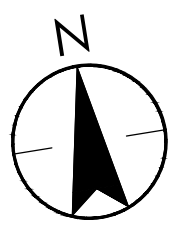
LEGEND

- | | | | |
|--|--|--|---|
| | SITE BOUNDARY | | PROPOSED ROAD CONTROL LINE |
| | EXISTING MAJOR CONTOURS & LABEL (1.0m INTERVAL) | | PROPOSED RETAINING WALL & HEIGHT |
| | EXISTING MINOR CONTOURS & LABEL (0.25m INTERVAL) | | PROPOSED MOUNTABLE KERB & CHANNEL |
| | PROPOSED MAJOR CONTOURS & LABEL (1.0m INTERVAL) | | PROPOSED BATTER TOP |
| | PROPOSED MINOR CONTOURS & LABEL (0.25m INTERVAL) | | PROPOSED CONCRETE DRIVEWAY - BY DEVELOPER |
| | ENVIRONMENTAL PROTECTION ZONE | | INDICATIVE CONCRETE DRIVEWAY - BY OWNER/BUILDER |
| | ADJACENT SITE - INDICATIVE KERB & CHANNEL | | |
| | EARTHWORKS PAD BOUNDARY | | |
| | EARTHWORKS PAD LEVEL | | |
| | PROPOSED SURFACE FLOW DIRECTION | | |

PLAN

1:400 (A1)

1:400 4 0 4 8 12 16 20 A1
1:800



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SUBDIVISION DEVELOPMENT APPLICATION

Title
CONCEPTUAL EARTHWORKS
SHADING PLAN

Project No.
301050923

Revision
B

Scale
1:400 (A1)

Drawing No.

CI-DA-100-P02

LEGEND

- SITE BOUNDARY
- EXISTING MAJOR CONTOURS & LABEL (1.0m INTERVAL)
- EXISTING MINOR CONTOURS & LABEL (0.25m INTERVAL)
- PROPOSED MAJOR CONTOURS & LABEL (1.0m INTERVAL)
- PROPOSED MINOR CONTOURS & LABEL (0.25m INTERVAL)
- ENVIRONMENTAL PROTECTION ZONE
- ADJACENT SITE - INDICATIVE KERB & CHANNEL

- PROPOSED ROAD CONTROL LINE
- PROPOSED RETAINING WALL
- PROPOSED MOUNTABLE KERB & CHANNEL
- PROPOSED BATTER TOP
- EARTHWORKS PAD BOUNDARY
- EARTHWORKS PAD LEVEL
- PROPOSED SURFACE FLOW DIRECTION

CUT/FILL LEGEND

- AREA OF CUT DEPTH > 2.5m
- AREA OF CUT DEPTH 2.0m - 2.5m
- AREA OF CUT DEPTH 1.5m - 2.0m
- AREA OF CUT DEPTH 1m - 1.5m
- AREA OF CUT DEPTH 0.5m - 1.0m
- AREA OF CUT DEPTH 0m - 0.5m
- AREA OF NO CUT & FILL
- AREA OF FILL DEPTH 0m - 0.5m
- AREA OF FILL DEPTH 0.5m - 1.0m
- AREA OF FILL DEPTH 1.0m - 1.5m
- AREA OF FILL DEPTH 1.5m - 2.0m
- AREA OF FILL DEPTH 2.0m - 2.5m
- AREA OF FILL DEPTH > 2.5m

EARTHWORKS VOLUMES

TOTAL CUT -14332m³
TOTAL FILL 2171m³
TOTAL BALANCE -12161m³ CUT

- INDICATIVE VOLUME ONLY:
- ASSUMES PAD LEVELS AS SHOWN
 - FROM DESIGN SURFACE LEVEL TO EXISTING SURFACE LEVEL WITH NO ALLOWANCE FOR PAVEMENT BOX, BULKING OR COMPACTION FACTORS.



PLAN
1:400 (A1)

J.BENTLEY

26711 2026.07.14
Authorised RPEQ # YYYY.MM.DD

FOR & ON BEHALF OF STANTEC AUSTRALIA PTY LTD

B	IR RESPONSE	SSBj	JB	2024.07.07
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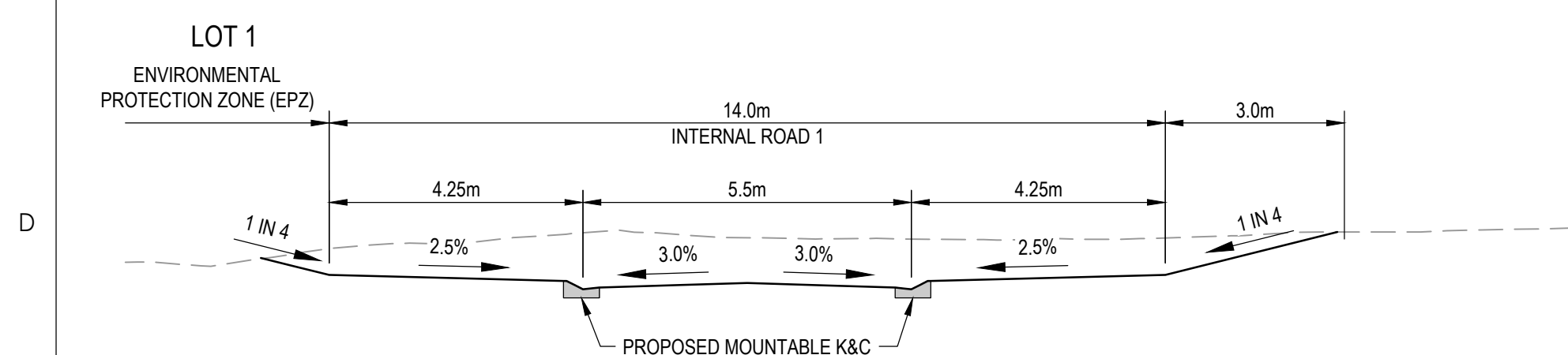
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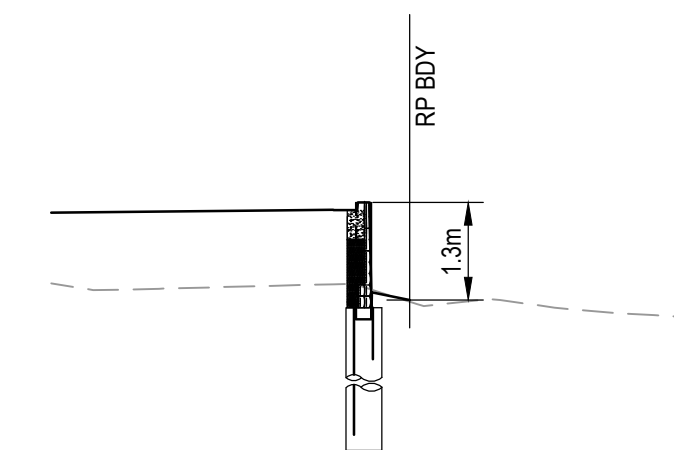
SUBDIVISION DEVELOPMENT APPLICATION

Title EARTHWORKS SECTIONS

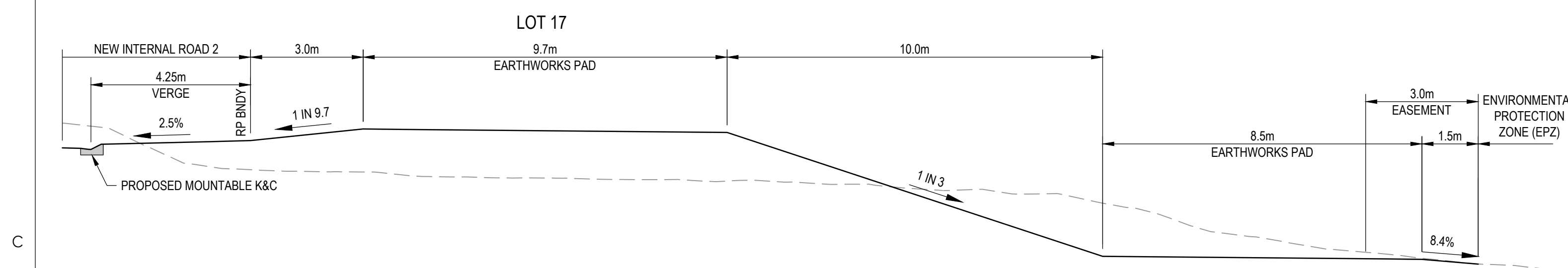
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Revision B Drawing No. CI-DA-103-C01



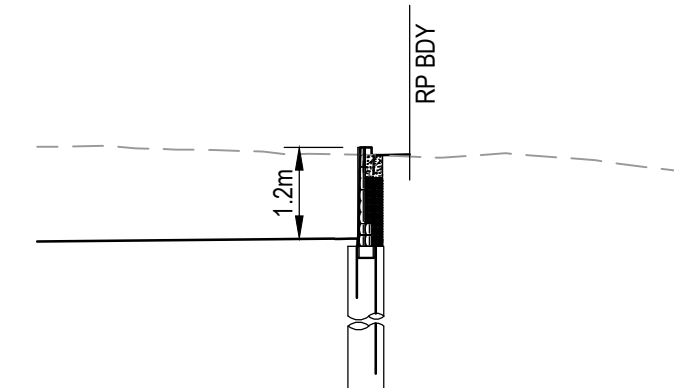
SECTION D
1:100 (A1)



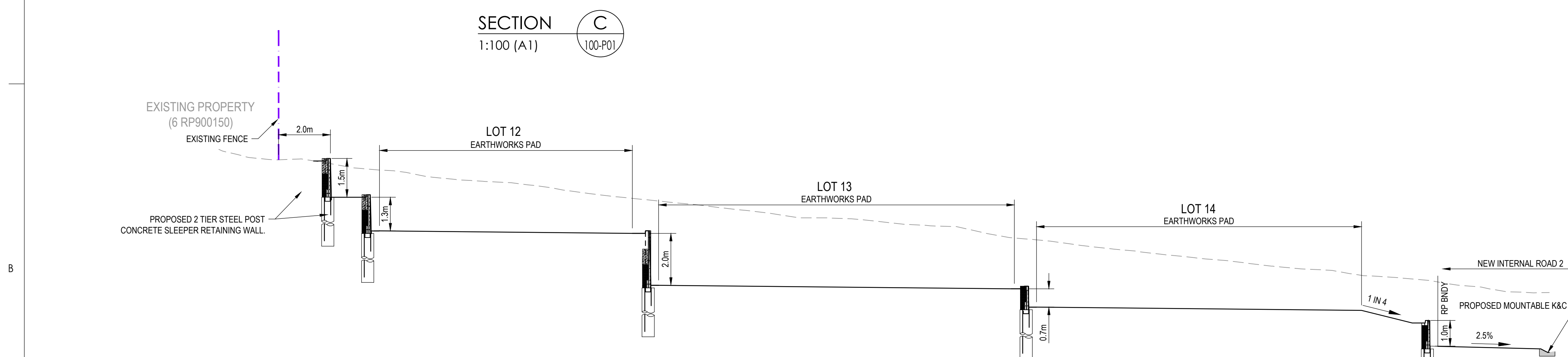
SECTION F
1:100 (A1)



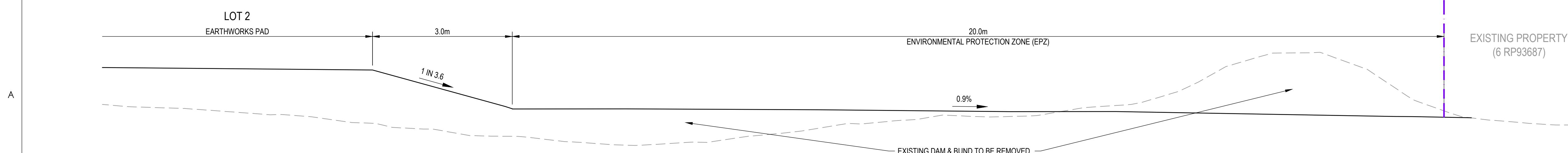
SECTION C
1:100 (A1)



SECTION E
1:100 (A1)



SECTION B
1:100 (A1)



SECTION A
1:50 (A1)

LEGEND

---	SITE BOUNDARY
---	NATURAL SURFACE
---	DESIGN SURFACE

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Notes

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Authorised RPEQ # YYYY.MM.DD
FOR & ON BEHALF OF STANTEC AUSTRALIA PTY LTD

B	IR RESPONSE	SSBj	JB	2026.07.07
A	CONCEPT DESIGN	JB	JB	2024.12.20
Issued/Revision		By	Appd	YYYY.MM.DD
File Name: 301050923-Cl-DA-106-D01.DWG		DDJ	LZ/KJ	HM/JB
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Issue Status

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341-349 BECKETT ROAD, BRIDGEMAN DOWNS

SUBDIVISION DEVELOPMENT APPLICATION

Title EARTHWORKS DETAILS

Project No. 301050923 Scale N.T.S.
Revision B Drawing No. CI-DA-106-D01

SLEEPER WALL NOTES

- DESIGN IS BASED UPON:
EARTH-RETAINING STRUCTURES CODE AS 4678-2002 CONCRETE STRUCTURES CODE AS3600-2001
BUILDING CODE OF AUSTRALIA
PROTOTYPE TESTING AT THE UNIVERSITY OF QUEENSLAND
- LAND SLIPS (e.g. SLIP CIRCLE FAILURE MECHANISMS) HAVE NOT BEEN CONSIDERED IN THE RETAINING WALL DESIGN. THE DESIGN IS BASED UPON THE ASSUMPTION THAT THE WALL IS FOUNDED ON GROUND NOT SUBJECT TO SLIP. THE DETERMINATION OF THE SITE SUSCEPTIBILITY TO SLIP IS THE RESPONSIBILITY OF THE CERTIFYING STRUCTURAL ENGINEER AND THE GEOTECHNICAL CONSULTANT AND IS BEYOND THE SCOPE OF WORK OF THIS DESIGN.
- RETAINING WALL DESIGNED FOR 5kPa IMPOSED LOAD BEHIND WALL.

DESIGN PARAMETERS

REFER DESIGN CERTIFICATION FOR RETAINED AND FOUNDING SOIL DESIGN PARAMETERS, DESIGN SURCHARGE LOADS and WALL DESIGN GEOMETRY

CONCRETE STANDARD - AS3600 STANDARD DESIGN EXPOSURE CLASSIFICATION SLEEPERS & POSTS - B1, FOOTINGS - A2

ELEMENT	GRADE	SLUMP	MIN. COVER
SLEEPER	N50	50mm	20mm **
FOOTING	N25	80mm	75mm

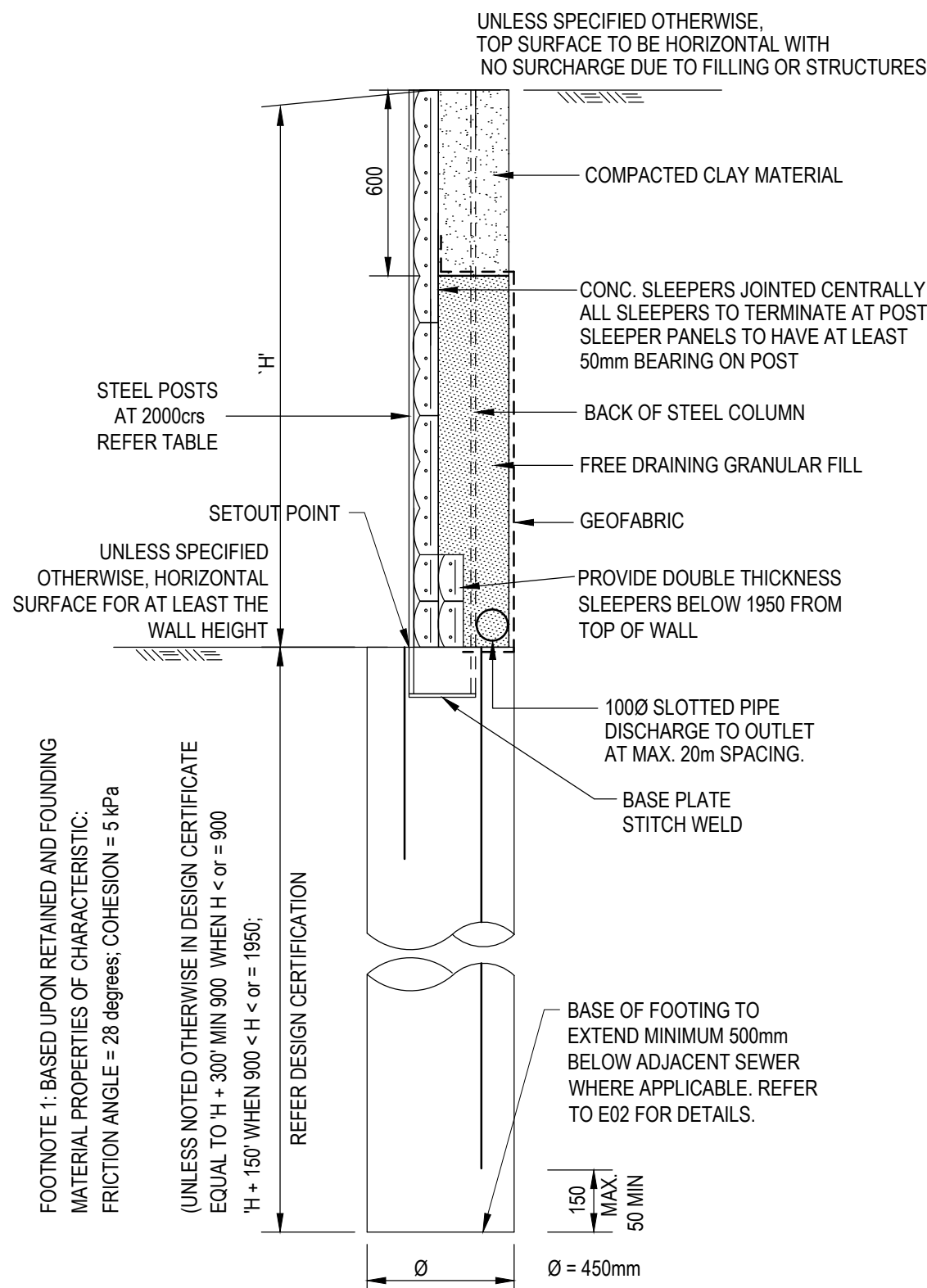
* RIGID FORMWORK & INTENSE COMPACTION
** MINIMUM SPECIFIED COVER 25mm.

REINFORCEMENT STANDARD - AS/NZS 4671 YIELD 500 MPa; DUCTILITY CLASS N

RETAINING WALL PROFILES SHOWN ARE INDICATIVE ONLY AND THE CONTRACTOR IS TO ENGAGE A GEOTECHNICAL AND STRUCTURAL ENGINEER TO DESIGN AND CERTIFY THE WALL PROFILE TO SUIT THE INSITU SOIL CONDITIONS, THE INTENDED LOCATION AND USE AND THE LOAD SURCHARGES THAT WOULD BE APPLICABLE TO THIS LOCATION. THE CONTRACTOR IS TO ENSURE THEIR CONSULTANT PROVIDES FULL CONSTRUCTION CERTIFICATION DEMONSTRATING COMPLIANCE WITH THEIR CERTIFIED DESIGN.

FOOTNOTE 1: BASED UPON RETAINED AND FOUNDING MATERIAL PROPERTIES OF CHARACTERISTIC:
FRICTION ANGLE = 28 degrees; COHESION = 5 kPa

(UNLESS NOTED OTHERWISE IN DESIGN CERTIFICATE EQUAL TO H + 300 MIN 900 WHEN H < or = 900
H + 150 WHEN 900 < H < or = 1950;
REFER DESIGN CERTIFICATION

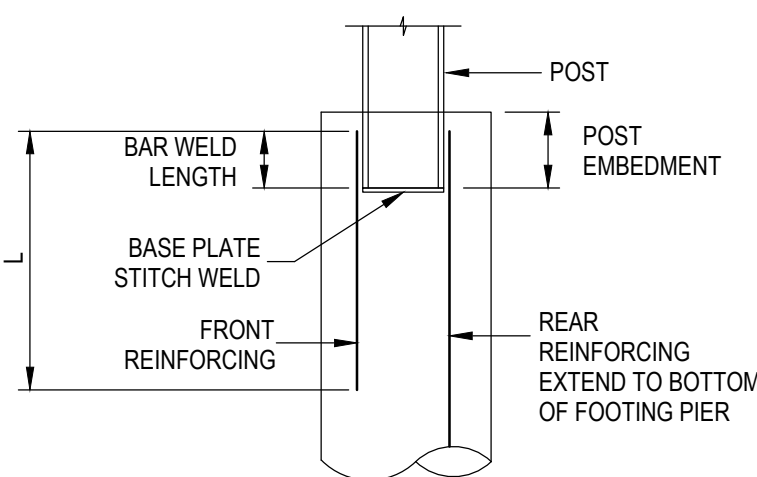


TYPICAL I-BEAM STEEL POST SLEEPER WALL

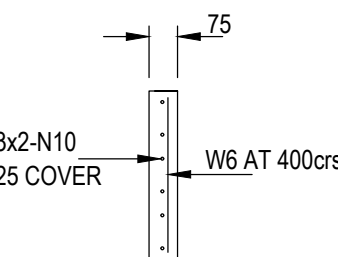
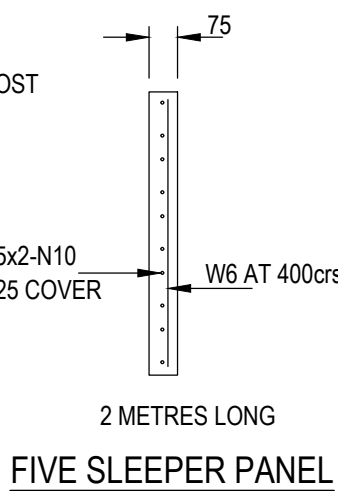
N.T.S.

INDICATIVE 2 TIER RETAINING WALL DETAIL

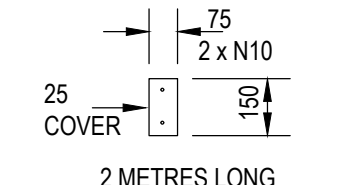
N.T.S.



TYPICAL POST BASE DETAIL
(H = 0-1950)



THREE SLEEPER PANEL
FOUR AND SIX SLEEPER PANELS SIMILAR



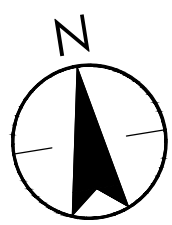
SINGLE SLEEPER PANEL

NOTE:
H.D. GALVANISE ALL STEELWORK
POSTS AFTER FABRICATION

* NOTE:
LIGATURE REQUIRED AT
BASE AS PER DETAIL ABOVE

I-BEAM SLEEPER WALL POST BASE DETAILS AND REINFORCING TABLE
(SINGLE HEIGHT WALLS)

RETAINING WALL MAX. HEIGHT 'H' (mm)	GALV. POST SIZE	POST EMBEDMENT (mm)	BASE PLATE (mm)	FRONT REINFORCING	FRONT REINFORCING LENGTH (L)	REAR REINFORCING	WELD LENGTH (mm)
600	100 UC 15	150	6	-	-	2/N12 6CFW ONE SIDE OF EACH BAR	100
900	100 UC 15	150	6	2/N16 6CFW ONE SIDE OF EACH BAR	700	2/N16 6CFW ONE SIDE OF EACH BAR	100
1350	100 UC 15	150	6	2/N16 6CFW BOTH SIDES OF EACH BAR	900	2/N16 6CFW BOTH SIDES OF EACH BAR	100
1950	200 UB 22	200	10	2/N20 6CFW BOTH SIDES OF EACH BAR	1100	2/N20 6CFW BOTH SIDES OF EACH BAR	150
2400	250 UB 26	250	12	2/N24 6CFW BOTH SIDES OF EACH BAR	1400	2/N24 6CFW BOTH SIDES OF EACH BAR	200
2700	250 UB 31	300	16	2/N28 6CFW BOTH SIDES OF EACH BAR	1650	2/N28 6CFW BOTH SIDES OF EACH BAR	250



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Notes

			
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A	CONCEPT DESIGN	DDJ/JT	HS 2024.12.20
Issued/Revision		By	Appd YYYY.MM.DD
File Name: 301050923-Cl-DA-400-P01.DWG		DDJ	LZ/KJ HW/JB
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341-349 BECKETT ROAD, BRIDGEMAN DOWNS

SUBDIVISION DEVELOPMENT APPLICATION

Title ROADWORKS & STORMWATER LAYOUT PLAN

Project No.
301050923

Revision
B

Scale
1:400 (A1)

Drawing No.

CI-DA-400-P01

STORMWATER NOTES

- DRAWINGS ARE CONCEPTUAL ONLY AND HAVE BEEN PREPARED AS AN ILLUSTRATION OF THE CONCEPTS DISCUSSED WITHIN THIS REPORT AND ARE INTENDED FOR DEVELOPMENT APPROVAL PURPOSES ONLY.
- WHERE LEVELS OR SIZES OF ANY WORKS ARE SHOWN, THESE ARE INDICATIVE ONLY TO DEMONSTRATE THE CAPABILITY OF THE SERVING OPTION PROPOSED AND ARE SUBJECT TO DETAILED DESIGN (OPERATIONAL WORKS DESIGN). THIS IS TO BE UNDERTAKEN IN ACCORDANCE WITH THE LATEST VERSIONS OF THE AUTHORITY STANDARDS, AUSTRALIAN STANDARDS AND OTHER INDUSTRY REFERENCE DOCUMENTS AT TIME OF DESIGN.
- DRAWINGS ARE NOT INTENDED TO BE USED FOR TENDER, ESTIMATING OR CONSTRUCTION.
- STORMWATER TREATMENT AND DETENTION SYSTEMS ARE CONCEPTUAL ONLY AND SUBJECT TO THE SPECIFIC DESIGN REQUIREMENTS OF THE LOCAL AUTHORITY, QUDM AND HEALTHY WATERWAYS.

LEGEND

--- SITE BOUNDARY

--- 39.0 --- EXISTING MAJOR CONTOURS & LABEL (1.0m INTERVAL)

--- 38.5 --- EXISTING MINOR CONTOURS & LABEL (0.25m INTERVAL)

--- 39.0 --- PROPOSED MAJOR CONTOURS & LABEL (1.0m INTERVAL)

--- 38.5 --- PROPOSED MINOR CONTOURS & LABEL (0.25m INTERVAL)

ENVIRONMENTAL PROTECTION ZONE

ADJACENT SITE - INDICATIVE KERB & CHANNEL

--- PROPOSED ROAD CONTROL LINE

--- PROPOSED RETAINING WALL

--- PROPOSED MOUNTABLE KERB & CHANNEL

--- PROPOSED BATTER TOP

--- PROPOSED CONCRETE DRIVEWAY - BY DEVELOPER

--- INDICATIVE CONCRETE DRIVEWAY - BY OWNER/BUILDER

--- PROPOSED CONCRETE FOOTPATH

--- PROPOSED NEW PAVEMENT

--- INDICATIVE BIN PAD

--- PROPOSED SURFACE FLOW DIRECTION

--- EXISTING STORMWATER LINE

--- PROPOSED STORMWATER LINE

--- PROPOSED ROOFWATER LINE

--- PROPOSED KERB ADAPTOR /STORMWATER GULLY / MANHOLE / PIT

--- PROPOSED BIORETENTION POD

--- PROPOSED WSUD STREET TREE POD

--- ADJACENT SITE - STORMWATER LINE

PLAN

1:400 (A1)

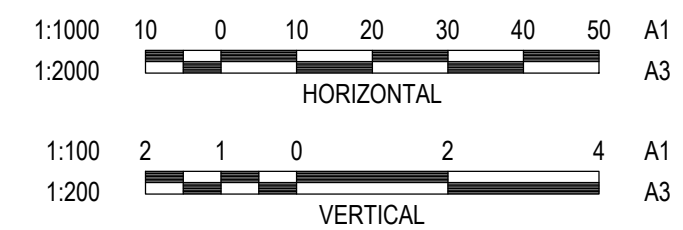
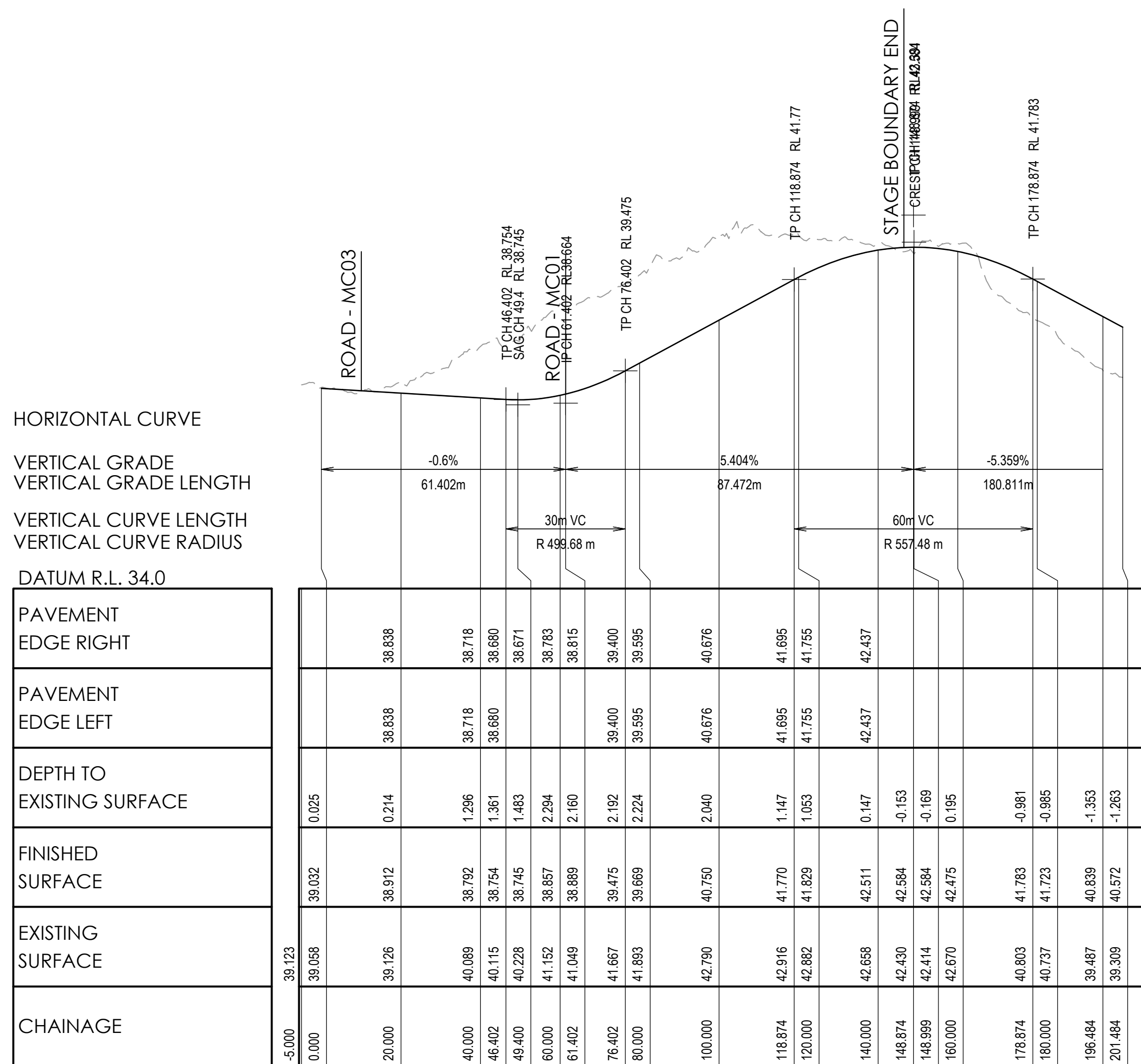
1:400 4 0 4 8 12 16 20 A1
1:800 A3

D

C

B

A



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26711 2026.07.14

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	File Name: 301050923-CI-DA-400-I02.DWG	DDJ	LZ/KJ	HM/JB
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				YYYY.MM.DD

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341-349 BECKETT ROAD, BRIDGEMAN DOWNS

SUBDIVISION DEVELOPMENT APPLICATION

Title ROADWORKS
LONGITUDINAL SECTION
MC02

Project No.
301050923

Scale
1:400 (A1)

Revision
A

Drawing No.

CI-DA-400-L02

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Notes

[illegible]

CHAINAGE 97.383

[illegible]

CHAINAGE 80.000

[illegible]

CHAINAGE 60.000

[illegible]

CHAINAGE 50.700


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341-349 BECKETT ROAD, BRIDGEMAN DOWNS

SUBDIVISION DEVELOPMENT APPLICATION

Title	ROADWORKS CROSS SECTIONS MC01
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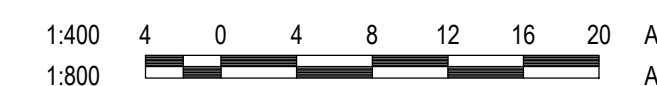
Project No.
301050923

Scale
1:400 (A1)

Revision
A

Drawing No. _____

CI-DA-400-C01



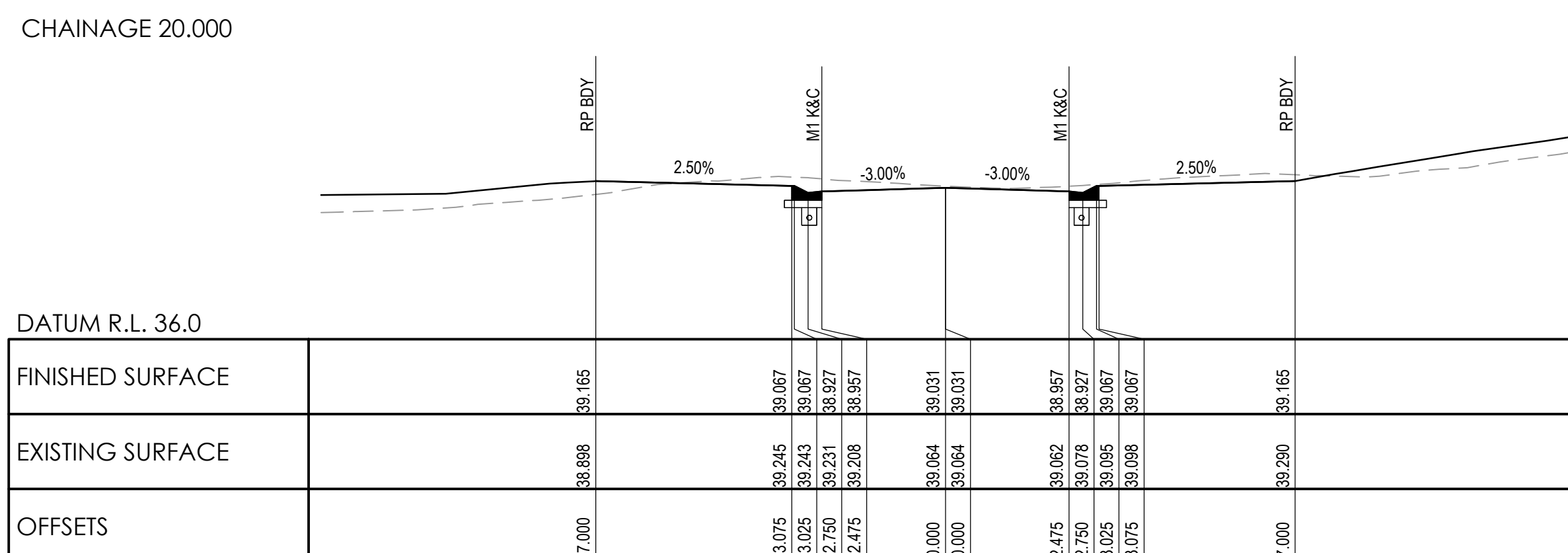
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SUBDIVISION DEVELOPMENT APPLICATION

Title	ROADWORKS CROSS SECTIONS MC02
-------	-------------------------------------

Project No.
301050923R
A

Drawing No. _____

Scale
1:400 (A1)

CI-DA-400-C02

CI-DA-400-C02



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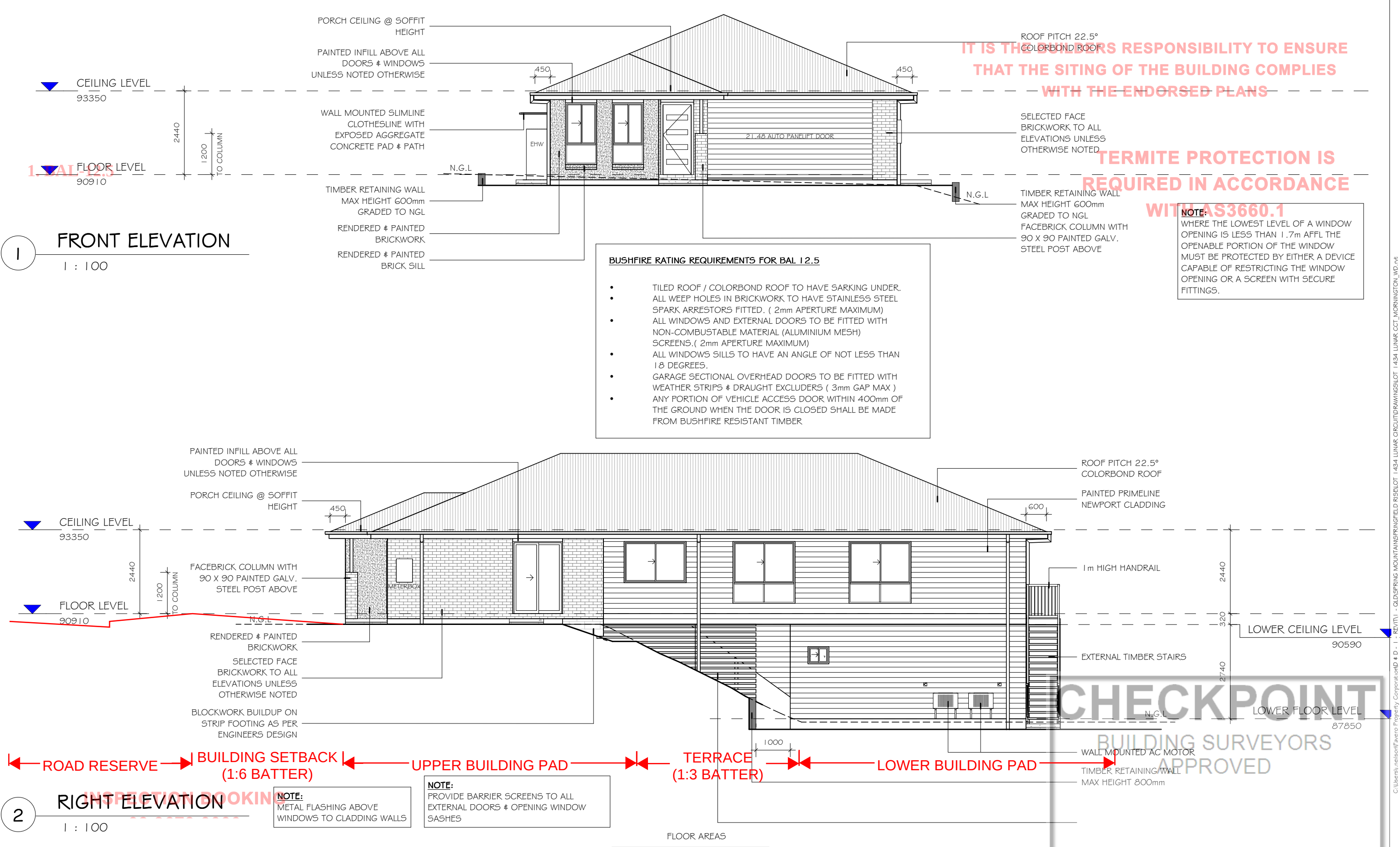
South Brisbane QLD 4101

Tel: +61 7 3811 4500

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Attachment 2 – Example Terraced Building Design



TERRACED HOUSE EXAMPLE WITH ACCESS FROM ROAD RESERVE TO UPPER STOREY.

NOTE: EXAMPLE BUILDING CONFIGURATION IS CONCEPTUAL ONLY, FINAL REQUIREMENTS ARE SUBJECT TO SITE SPECIFIC DESIGN BY ACCREDITED BUILDER AND TO RELEVANT APPROVALS.